

East Gateway Specific Plan

Santa Paula, California



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East Gateway Specific Plan

Santa Paula, California

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Chapter 1: Introduction

1.1 Introduction

1.1 Introduction

The East Gateway Specific Plan Area (“Plan Area”) is located in Ventura County, California at the eastern edge of the City of Santa Paula (Figures 1-1 and 1-2). The City of Santa Paula is generally located along the north bank of the Santa Clara River, bisected by State Route (SR) 126, approximately 7 miles west of the City of Fillmore, and 7 miles east of the City of San Buenaventura.

The East Gateway Specific Plan Area includes approximately 36.5 acres, bounded by SR 126 to the north, agricultural land and Haun Creek to the east, the existing Lemonwood Industrial Park and the Santa Clara River to the south, and Hallock Drive and a neighboring industrial parcel to the west. Primary access to the Specific Plan Area from the surrounding region is provided by SR 126 and Hallock Drive.

The City of Santa Paula is located in the center of the Santa Clara River Valley. The historical significance of the citrus industry led the City to be known as the Citrus Capital of the World.

Santa Paula’s history can be traced through its diverse residential neighborhoods, classic small town downtown district, and the variety of barns, packing houses and other simple agrarian and utilitarian structures that contribute to its unique heritage as the commercial center of this agricultural valley. As directed by the General Plan - which states that this portion of East Area 2 should serve as a graceful transition from the more urban character of the town to the rural character of the adjoining greenbelt and countryside - the design character of the East Gateway Specific Plan Area is a composite of small town, rural and highway commercial design elements and characteristics.

As further directed by the City’s General Plan, this Specific Plan provides for a mix of retail, service and light industrial and office uses, designed and scaled for compatibility with Santa Paula’s small town image and identity. This Specific Plan includes comprehensive development standards, guidelines and implementation measures to ensure the creation of a well-organized, flexible and attractive highway-oriented district at the East Gateway to Santa Paula.

Figure 1-1: Regional Location Map

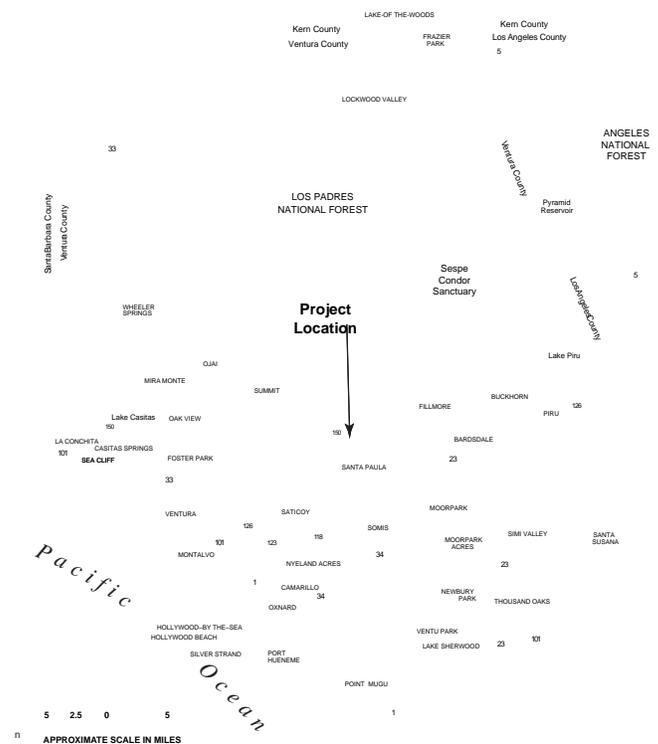


Figure 1-2: Specific Plan Area Location



Chapter 1: Introduction

1.2 Specific Plan Area

1.2 Specific Plan Area

The East Gateway Specific Plan Area is located within the East Area 2 Planning Area, as identified in the City of Santa Paula General Plan, approximately 136 acres based on the general level of mapping completed for the General Plan Update. The East Gateway Specific Plan Area is located at the southeasterly corner of East Area 2. Figure 1-3 shows the Plan Area and its immediate context.

The East Area 2 Planning Area lies outside of the City's jurisdictional boundaries. This Specific Plan regulates certain portions of the EA2 Planning Area in addition to three parcels that are already located within the City's boundaries (and are not part of the EA2 Planning Area). To ensure the orderly development of East Area 2 and East Area 1 to the north, the City is seeking the annexation of the portion currently outside the city limits concurrently with the consideration and adoption of this Specific Plan.

Because the entitlement process for the Specific Plan includes annexation of property into the City. The Santa Paula General Plan is the applicable document with which the Specific Plan must comply. Accordingly, this Specific Plan treats the City of Santa Paula General Plan as the document of record. More detail about the General Plan status of the Plan Area is provided in the following sections.

As shown in Figure 1-4, the Specific Plan Area includes 5 existing undeveloped parcels and the portion of SR 126 located between these parcels. Four of these parcels are located south of SR 126 and one parcel is located north of SR 126. Of the four parcels to the south of SR 126, the westerly three, totaling 7 approximately acres, lie within the current City limits. A portion of these three parcels is currently used of outdoor storage of equipment and materials associated with the existing industrial facility to the south. The fourth parcel, approximately 25 acres in size, lies outside the current City limits. This parcel is vacant.

After adoption of this Specific Plan, and annexation to the City, the zoning of the entire Specific Plan Area will be designated East Gateway Specific Plan, as defined herein. This designation is very similar in land use intent and intensities to the City's Highway Commercial (CH) and Light Industrial

Figure 1-3: Oblique View of the Specific Plan Area and Vicinity



Figure 1-4: Overhead View of the Specific Plan Area and Vicinity



*Santa Paula City Limits
2007 Santa Paula Sphere of Influence
2008 CURB Boundary*

(LI) zones, adjusted to allow and organize a mix of commercial and light industrial uses, and to ensure that the scale and character of the development is consistent with the property's unique "edge of town" location and Santa Paula's small town character.

1.3 Relationship to Santa Paula General Plan

A specific plan is a tool for implementation of the General Plan and, therefore, must be consistent with the City's General Plan (Government Code §§65450, *et. seq.* - see Figure 1-5). The General Plan Consistency Review in Appendix B (General Plan Consistency Review), documents the consistency of the East Gateway Specific Plan with the Santa Paula General Plan. The Specific Plan also contains all information required by State law as identified in Figure 1-5, to the right.

The Santa Paula General Plan identifies East Area 2 - of which the Specific Plan Area is a part - as a Planning Area anticipated to be urbanized and developed. Section III of the Santa Paula General Plan Land Use Element - pages LU-18 to 25 - discusses this area, along with other areas identified as "Expansion Areas," and provides specific direction with regard to the key land use designations and intensities and urban design objectives for East Area 2 as summarized in the following section.

The City has determined that preparation and adoption of a specific plan is appropriate to guide the development of this key area. Development of this area is anticipated within the next five years, concurrent with development beginning in East Area 1 to the immediate north. This Specific Plan provides for the type and intensity of land use allowed by the General Plan in East Area 2, reflects relevant development policies as described in the General Plan, and provides appropriate direction regarding site design, infrastructure, provision of open space, and implementation measures.

Figure 1-5: Specific Plan Requirements

Specific Plan REQUIREMENTS
CALIFORNIA GOVERNMENT CODE
SECTION 65450

Government Code §§ 65450 *et seq.* requires the following in a specific plan:

65450. After the legislative body has adopted a general plan, the planning agency may, or if so directed by the legislative body, shall, prepare Specific Plans for the systematic implementation of the general plan for all or part of the area covered by the general plan.

65451. (a) A Specific Plan shall include a text and a diagram or diagrams which specify all of the following in detail:

- (1) The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
- (2) The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
- (3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
- (4) A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out paragraphs (1), (2), and (3).

(b) The Specific Plan shall include a statement of the relationship of the Specific Plan to the general plan.

1.3.1 General Plan Description of East Area 2

The General Plan identifies East Area 2 as a Planning Area in the southeastern corner of the city. East Area 2 is identified as 136 acres, of which 50 acres area intended to accommodate Highway Commercial uses, 55 acres Light Industrial/R&D uses, and 31 acres Industrial uses. The General Plan Land Use Plan defines the maximum allowed intensity of these uses as: 544,500 square feet of Highway Commercial development, 718,740 square feet of Light Industrial/R&D development and 334,590 square feet of Industrial development.

The portion of the Specific Plan Area currently located in the City south of SR 126, and the parcel located north of SR 126 are currently designated Mixed Use Commercial/Light Industrial on the General Plan Land Use Plan.

Chapter 1: Introduction

1.3 Relationship to Santa Paula General Plan

1.3.2 Proposed Specific Plan

This Specific Plan defines development and design standards, as well as a flexible framework of circulation and infrastructure to support the types and intensities of uses identified in the General Plan. This planning framework consists of a clear urban design vision and related standards for the primary internal circulation spines, and for the four edges of the Specific Plan Area, each of which has a distinct design character. The design character responds to the direction of the General Plan - particularly the goals, objectives and policies of Land Use Element Section 5, Urban Form and Design - as described in more detail in Appendix B.

- The primary face of the Specific Plan Area is SR 126, and unique and specific design standards are provided for this important east gateway to the City of Santa Paula.
- The east edge of the Specific Plan Area abuts active agricultural land and the Santa Paula/Fillmore Greenbelt and the south edge abuts the Santa Clara River. As directed by best practices and the General Plan, appropriate transitions and landscaped buffers are defined.
- A portion of the western edge of the Specific Plan Area faces Hallock Drive, also an important entry to the City from Highway 126, and standards similar to those for the north frontage are provided. The balance of the west boundary abuts an existing industrial facility and appropriate standards for screening and service are provided.

The circulation framework for the Plan Area and design standards for each of these frontages will ensure that a unified and flexible mix of commercial and industrial businesses is served by appropriate access and parking, and that it appropriately connects to and contributes to the quality of, adjacent areas and environments. After the adoption of this Specific Plan and annexation of the easterly portion of the Plan Area, all new development proposals within the Plan Area must be found to be consistent with this Specific Plan.

1.4 Specific Plan Preparation

The preparation of the East Gateway Specific Plan was initiated by the Limoneira Company in consultation with the property owners to ensure the coordinated planning of this important east gateway to the City of Santa Paula as development occurs

within the next 5 years, concurrent with the development of the initial phases of the adjacent East Area 1 community. The East Area 1 Specific Plan was adopted by the City and approved through a public vote in June of 2008, followed by approval of the City's annexation of East Area 1 in March of 2011 by the Ventura County Local Agency Formation Commission (LAFCo).

Because a portion of the East Gateway Specific Plan Area is located in the unincorporated area of Ventura County, the County LAFCo must approve the annexation of this portion of the Specific Plan Area. Accordingly, the City of Santa Paula is processing a request to annex this area along with other unincorporated territory to the west, Commonly referred to as "East Area 2".

The Ventura LAFCO was formed and operates under the Cortese-Knox-Hertzberg Local Government Reorganization Act of 2000 (California Government Code Sections 56000 et seq.). State law provides for LAFCOs to be formed as independent agencies in each county in California. LAFCOs implement state law requirements and state and local policies relating to boundary changes for cities and most special districts, including spheres of influence and changes in organization.

1.5 Organization of the Specific Plan

The East Gateway Specific Plan is organized to include all required elements of a specific plan, including design standards. The Illustrative Plan presented in Figure 2-5 shows the site organization and general design intention. The Specific Plan is organized into five sections that address the various issues important to the planning of this area, and the topics required by the Government Code for specific plans.

- 1. Introduction.** The introduction describes the intent and purpose of this Specific Plan and the community and planning context for the East Gateway Area within the City. This includes an overview of the relationship of the Specific Plan to the Santa Paula General Plan, and existing market conditions and descriptions of the land use goals, policies and objectives for the East Gateway Area.
- 2. Vision, Form and Character.** This section describes and illustrates the intended form, character, and uses of the East Gateway development.
- 3. Development Code.** This section describes the physical

design of the East Gateway Specific Plan Area, including the layout and design of internal private streets, the organization of the site into blocks defined by those private streets, the location of the land uses planned for the area, and the development standards that will shape the form and character of new development. This Plan places a strong emphasis on the physical form and character of the development, as well as the uses within the buildings.

These development standards and design guidance are formatted as a graphically oriented "Regulating Code" which describes the intended urban and architectural design patterns, while also regulating the uses of the buildings and lots within the Plan Area. The Development Standards specify the development intensity that may be achieved under the Specific Plan. They also define and regulate the design of the open space network that forms the framework and provides the infrastructure for the East Area Gateway Plan Area, implementing a circulation network that encourages and balances the use of all travel modes, particularly pedestrians.

- 4. Infrastructure and Public Services.** This section provides a summary of the infrastructure systems and public services planned to support the uses permitted by the East Gateway Area.
- 5. Implementation.** The implementation section describes the procedures and processes for implementing this Specific Plan.

Appendix A: Definitions

Appendix B: General Plan Consistency Review

1.6 Overview of Existing Conditions

The Santa Paula General Plan plans for the growth of the City through the year 2020. As the majority of the 4.5 square miles that made up the City at the time the plan was prepared were developed, the General Plan identified several adjacent areas suitable to accommodate future growth. On the eastern edge of the City, these areas included the East Area 1 expansion area and the East Area 2 planning area. After adoption of the General Plan in 1998, the City obtained LAFCo approval of an amendment to the City's Sphere of Influence to include the East Area 2 Planning Area, followed by an amendment to the Santa Paula- Fillmore Greenbelt Agreement to remove East

Area 2 from the Greenbelt.

As described above, the East Area 1 Specific Plan was approved in 2008 and the annexation of this expansion area was approved by the LAFCo in early 2011. The East Area 1 community is master planned to include several residential neighborhoods that will include 1,500 residences, including multi-family apartments and condominiums and single-family houses. A wide variety of public facilities is also planned, including schools, and neighborhood and community parks. A small amount of neighborhood retail commercial uses are also permitted by the East Area 1 Specific Plan along Hallock Drive, and a small amount of business park uses are planned along the Santa Paula Branch Rail Line as these uses are compatible with rail operations. Development of East Area 1 will begin in the near future with the completion of this community over ten years.

The East Gateway Specific Plan Area is located immediately south of East Area 1 and provides land suitable for the development of a highway oriented retail center that can meet the needs of the existing community of Santa Paula and the new residential neighborhoods in East Area 1, and for additional business park uses that will provide jobs for residents of Santa Paula. Development of the initial phases of East Area 1 will include street, water, wastewater and other infrastructure improvements that will also facilitate development of the East Gateway Area. In response to market demand and with the benefit of the planned infrastructure improvements for East Area 1, the owners of the property in the East Gateway Specific Plan Area plan to develop the property within the next five years.

1.6.1 Demographics

According to the 2010 U.S. Census Data, Santa Paula's demographic information shows:

- The population increased 2.5% since 1990, from 28,598 in 1990 to 29,321 in 2010 (U.S. Census).
- A significant percentage of residents are Hispanic or Latino (over 79.5%) (U.S. Census).
- The homeownership rate (about 56.3%) slightly outpaces renter-occupied housing (47.3%) (U.S. Census).

Chapter 1: Introduction

1.6 Overview of Existing Conditions

- The average household size (3.5 people per household) is similar to adjacent cities and slightly higher than Ventura County (3.1 people per household) (U.S. Census).
- The City has experienced an increase in median income from \$45,419 in 1999 to \$50,870 in 2009 (U.S. Census Bureau).

Overall, Santa Paula is experiencing growth from a variety of household types that are attracted by the small town character of the community and attainable cost of housing relative to other cities in the area. This has significant implications for both the residential and commercial real estate markets, in that new development can serve a wide variety of households. First-time homebuyers include not only the more typical small households without children, but also households with large families that have been in the area for a significant amount of time.

1.6.2 Environmental Setting

The City of Santa Paula was founded around and continues to be largely sustained by agriculture and associated industries. The downtown commercial center of the City grew from the proximity to the railroad and the residential areas spread northward into the hills. The Santa Clara River and Santa Paula Creek acted as natural boundaries to the growth of the city.

Through the 2008 Plan Santa Paula process - and through a subsequent public vote on the East Area 1 Specific Plan in 2008 - the City determined that the preferred area for the next phase of the growth of the city as envisioned in the 1998 General Plan was eastward from Santa Paula Creek to Haun Creek, the planned easterly edge of the City of Santa Paula abutting the Santa Paula/Fillmore Greenbelt. The East Area 1 Specific Plan defines that eastern edge of Santa Paula to the north of the Santa Paula Branch Rail Line, and this East Gateway Specific Plan completes this edge along both sides of Highway 126.

North of State Route 126, the Specific Plan Area includes a small vacant parcel located east of existing commercial development and south of the Santa Paula Branch Rail Line. To the south of State Route 126, the East Gateway Specific Plan Area includes three other graded vacant parcels with frontage on S. Hallock Drive and State Route 126.

The remainder of the Specific Plan Area consists of a parcel used for growing a variety of row crops. This parcel slopes gently to the south from Highway 126. A small unnamed stream flows from drainage culverts under Highway 126 through the northerly portion of the Plan Area, receiving stormwater from a portion of East Area 1 to the north. This stream bounds much of the easterly edge of the Specific Plan Area, separating it from agricultural land to the east before joining Haun Creek which forms the remainder of the east boundary of the Specific Plan Area. The land to the south of the Specific Plan Area consists of undeveloped open space, with the main channel of the Santa Clara River located approximately one thousand feet to the south.

The Specific Plan Area overlies a portion of the Santa Paula and Fillmore groundwater basins. Water for the existing agricultural operations is provided by water wells located on and adjacent to the Specific Plan Area operated by Farmer's Irrigation Company, a local mutual water company.

1.7 Plan Goals, Policies and Objectives

Goal: A desirable outcome or future condition toward which the City will work.

Policy: A short statement intended to guide decision-making.

Objective: A measurable outcome that implements the intent of the Specific Plan.

Goals

The East Gateway Specific Plan has been prepared to promote and implement the following:

1. Establish a strong “East Gateway” to the City of Santa Paula that bridges between the small-town urban character of Santa Paula and the rural and agrarian character of the Santa Paula/Fillmore Greenbelt. Accomplish this with a composite of town-scale urban design, architecture and signage reflecting the agrarian and highway commercial heritage of the Santa Clara River Valley, and landscape emphasizing rural elements and detailing coordinated with the south edge of East Area 1 to the north.
2. Organize the Plan Area into a series of flexible “blocks” to organize the location of buildings, parking areas and open spaces, defined by a simple and flexible grid of private streets and drives accommodating vehicles, pedestrians and bicyclists.
3. Ensure that buildings with active, human-scale frontages face the internal street/drive and open space network, welcoming customers, employees and visitors.
4. Define simple buildings and flexible land use regulations to support a mix of retail, commercial and light industrial uses to provide goods, services and employment to the residents of Santa Paula and the Santa Clara River Valley.
5. Establish the “highway gateway” character along Highway 126 as it passes through the Plan Area through the use of landmark elements and buildings, and along with bold but appropriate commercial and civic signage and graphics that relate to Santa Paula’s agricultural roots and physical pattern, scale and character.
6. Support the fiscal and economic health of the City of Santa Paula by providing a regional-scale center for retail, commercial and employment uses on this unique, highway-facing property.
7. Support the conservation of natural resources through the appropriate design of the permanent urban edges abutting the Santa Paula/Fillmore Greenbelt and Santa Clara River, and by developing a commercial/industrial district that will provide new goods, services and jobs locally, reducing the need for City and area residents to commute out of the area.

Chapter 1: Introduction

1.7 Plan Goals, Policies and Objectives

Policies

- A. **Buildings along Highway 126 reflect Santa Paula's small town-scale and agrarian heritage.** Buildings along highway frontages are smaller than buildings farther from the highway, freestanding with four-sided architectural character, and with natural and agriculturally-oriented landscape interspersed to accent the buildings.
- B. **Plan Area is organized into "blocks" by a simple and flexible grid of private streets and drives.** Streets are balanced and scaled to their context and role for use by motorists while creating a low-speed environment suitable for pedestrians and bicycles.
- C. **Buildings face the streets with welcoming frontages and entries scaled to the pedestrian.** Facades are designed with shopfronts that can be used by a variety of commercial or retail businesses while generating an appealing streetscape.
- D. **Parking areas are organized within the block structure, connected to the buildings by walkways.** Parking is located conveniently and efficiently within a pattern that emphasizes the buildings, businesses and pedestrians, encouraging - but not requiring - shoppers to patronize multiple businesses without driving and re-parking. Landscaping emphasizes the private street and drive frontages with tree rows, with randomly located larger trees distributed within many parking areas - rather than the more conventional pattern of many smaller trees - reflect a more rural landscape character.
- E. **Plan area edges abutting the Greenbelt, creeks or river are landscaped with native plants to transition smoothly to those natural or agricultural environments.** Buildings and streets near the plan area's edges utilize special details for connecting the development sensitively with the adjacent areas to be left in a natural state. Landscape along these edges is rural and natural in character, emphasizing native and non-invasive plant materials that create a seamless transition to the adjoining natural and agricultural environments. The landscape palette along Highway 126 also emphasizes native plants to help integrate the new "edge of town" development into the adjoining rural landscapes.

Plan-Wide Objectives

In addition to the above plan-wide policies, the following objectives apply to the entire plan area:

1. Provide a variety of commercial, retail and limited industrial opportunities aimed at serving both the local and regional population.
2. Connect the plan area with Highway 126 and Hallock Drive.
3. Organize the plan area into an interconnected and flexible grid of blocks that accommodate a wide variety of building sizes, parking, defined by appropriate streetscapes.
4. Create appropriate frontages and transitions along the plan area's edges: Highway 126, Hallock Drive, Santa Paula/Fillmore Greenbelt, and Santa Clara River.
5. Achieve approximately 300,000 to 350,000 square feet of commercial, limited office and limited light industrial space.
6. Connect buildings to the internal private street network with frontages that support their intended use, whether office, light industrial or retail.
7. Design buildings to reflect Santa Paula's town-scale, agricultural roots and physical character through appropriate building scale, height, form and character. For large buildings in the southerly blocks, such scale and character may be provided primarily with facade articulation facing toward the highway.
8. Provide signage that meets the needs of the uses within the Specific Plan Area while contributing to the unique highway commercial character envisioned for Santa Paula's East Gateway.

Chapter 2: Vision, Form, and Character

2.1 Introduction

2.1 Introduction

The key design principles of the East Gateway Area are related to the core concept of providing the City of Santa Paula with a highway-oriented commercial and employment center of regional significance that defines its East Gateway, in a setting that reflects the City’s small-town image and character, and its location within one of California’s most important agricultural valleys. These design principles include:

- The Plan area is organized around a network of pedestrian-oriented streets that form it into 4 to 6 simple blocks.
- The Plan area can accommodate a range of uses that are accessible by auto, bike or foot, and which generally share a common supply of parking integrated into the block structure.
- Buildings - whether retail, service commercial or research and development in program - are simple in their massing,

detailing, and materials, face the internal streets with welcoming entries, and present a semi-rural “edge of town” character to SR 126.

- The landscape is simple and rural in character, characterized by rows or clusters of large trees common to the rural landscapes of the region; walls, fences and pavements of rustic, natural materials; and drought-tolerant and native ground plantings that reinforce the rural agricultural landscape character.

This Section of the Specific Plan describes the intended form, character, and uses of East Gateway, and introduces the Development Standards – detailed in Section 3 – that implement the policies described in the previous section. These policies are the foundation for planning and design of the plan for the East Gateway Plan Area, and are the basis for the Development Standards.

Figure 2-1: Design Intent for SR 126 Edge of EA-2 Site



Massing model and precedent photos illustrating design intent for the SR 126 edge of the project site.

Chapter 2: Vision, Form, and Character

2.2 Development Program and Framework

2.2 - Development Program and Framework

Based on the unique location of the Plan Area - at the City's eastern gateway and the only large underdeveloped site in Santa Paula with direct access to SR 126 - and based on the General Plan's designation of this area for Highway Commercial use, the primary land use alternative for the site is a regional scale retail and commercial center. Given the site's approximately 36.5 [including the area of SR 126 between blocks 2 and 7] acre size and the General Plan's anticipation of a floor area ratio (FAR) of up to 0.25 for such uses, a retail center (Regional Retail Alternative) of up to 380,000 s.f. would be appropriate. Such a center would provide a range of economic, fiscal and quality of life benefits to the City of Santa Paula.

The General Plan also anticipates the possibility of light industrial and research and development (R&D) uses up to an FAR of 0.30 in this area. That alternative could yield an employment center (Employment Center Alternative) of up to 460,000 s.f. Such a district could provide significant economic development, employment and fiscal benefits to the City. Another scenario includes more retail and service commercial uses near the SR 126 frontage and more industrial uses toward the south edge of the Plan Area, and that is defined as the "Mixed-Use Alternative".

Accordingly, this Plan provides a framework of infrastructure and urban design that will accommodate any of these development programs in an "edge of town" setting consistent with Santa Paula's small-town character.

The Illustrative Plan (Figure 2-1) is a graphic representation of the essential form that the East Gateway is intended to take. The major elements of the Plan include:

- Three primary private streets, "A" running south from SR 126 to the southerly portion of the property "B" running east from Hallock Drive connecting to the easterly portion of the property, and "C" running parallel to "B" approximately 400 feet to the south.
- Flexible additional north-south "drives" providing access to additional buildings, and an east-west "service drive" along the southerly edge of the property.
- These private streets and drives generally organize the Plan Area into 6 blocks, within which buildings, open spaces and parking areas can be flexibly accessed by vehicles and pedestrians, with service access primarily organized along the southerly and westerly edges of the site.

Through the process of site analysis and conceptual design, it is clear that a combination of factors - the irregular shape of the site, the CalTrans setback areas from SR 126 and the open space resources to the east and south, and the parking ratios necessary to support the intended program - reduce the achievable building area for the Regional Retail alternative to approximately 300,000 s.f. The achievable building area for the Mixed-Use Employment District program is estimated to be 350,000 s.f. See Table 2-1 below.

Table 2-1: Buildout Table

Block	Acres	Regional Retail Center			Mixed-Use Employment District		
		Use	Building Area	F.A.R	Use	Building Area	F.A.R
1	4.2	Retail	25,000	0.14	Retail / Service	25,000	0.14
2	5.9	Retail	31,000	0.12	Retail / Service	30,000	0.12
3	6.0	Retail	60,000	0.23	R&D	75,000	0.29
4	3.2	Retail	20,000	0.14	R&D	45,000	0.32
5	5.7	Retail	64,000	0.26	R&D	80,000	0.32
6	7.5	Retail	100,000	0.31	R&D	95,000	0.29
7	2.1	Retail	10,000	0.10	Retail	10,000	0.10
	36.4		310,000	0.18		360,000	0.23

Figure 2-1: Illustrative Master Plan - Regional Retail Program



Chapter 2: Vision, Form, and Character

2.2 Development Program and Framework

2.2.1 - Regional Retail Center Alternatives

The Illustrative Plan on the previous page and Figure 2-2 illustrate both the idealized organization of the Plan Area and a likely layout for the Regional Retail program. Primary access is provided to the center from both SR 126 and Hallock Drive, distributing on-site traffic throughout the site via Streets A, B and C, two Retail Drives and an Edge Drive as shown.

Buildings are sited fronting these on-site Streets and Drives with shopfronts and signage. With the potential exception of the Edge Drive, these private streets are typically provided with sidewalks on at least one side so that it is convenient for customers to park once and visit multiple shops. This helps to create a comfortable small-town character for a shopping center that is programmatically quite typical of those serving a regional trade area.

The illustrative plan in Figure 2-3 shows a variation of the Regional Retail Center program, in which Street A is located farther to the east. This variation might be preferable if a) CalTrans required the main entry point from SR 126 to be located farther to the east than shown in figure 2-2, or b) if the anchor retail tenants preferred this type of parking layout with no central drive.

For either of these layout options, or a variation on these, the intent is to locate smaller restaurant or retail buildings within Blocks 1 and 2, and potentially along the north edge of Blocks 3 and 4, with larger retail tenants located along the westerly edge of Block 3 and the southerly edges of Blocks 5 and 6. This programmatic alternative responds to General Plan priorities including:

- Providing convenient commercial goods and services for the employment districts within East Area 2 and the neighborhoods of East Area 1;
- Providing new local jobs and sales tax generation to improve the City's fiscal sustainability, jobs/housing balance, and local economy.
- Developing an expansion area by extending the simple grid structure and small-town character of Santa Paula.

Figure 2-2: Regional Retail Program - Alternative A



Figure 2-3: Regional Retail Program - Alternative B



2.2.2 - Mixed-Use Alternatives

Figure 2-4 illustrates a Mixed-Use Program alternative developed within that same block pattern shown in Figure 2-2. In this alternative, smaller restaurant and/or commercial retail or service businesses are organized in Blocks 1 and 2, taking advantage of visibility and pass by traffic from SR 126, while Blocks 3, 4, 5 and 6 are developed with a combination of office, R&D and Light Industrial uses.

Figure 2-5 illustrates a variation on the same Mixed-Use Program, with Street A angled to the east to adjust the location of the intersection with SR 126, and to potentially better accommodate the phased development of the site according to existing ownership interests.

This alternative also illustrates the possible orientation of smaller restaurant “pad buildings” more toward Street A and less to SR 126, which has the potential to better define the character of Street A. If this option is pursued, a strong highway signage strategy - which could be very much in keeping with the “edge of town highway” character of the site would likely be necessary for those businesses oriented to Street A. The general configuration of Blocks 1 and 2 shown in Figure 2-5 would be very appropriate for either variation of the Retail Regional Center alternatives illustrated in Figures 2-2 and 2-3.

2.2.3 - R&D/Light Industrial Alternative

A purely R&D and light industrial program for this highway oriented site is not considered likely. However, it is accommodated for by this Specific Plan if the demand for Employment District uses should be stronger than for Regional Retail or community-serving retail. Such a scenario would be essentially the same as the plan illustrated in Figure 2-5, with the buildings organized in Blocks 1 and 2 (potentially with minor recalibrations of size and orientation) simply changing their use from Regional Retail to R&D and light industrial.

Figure 2-4: Mixed-Use Program - Alternative C



Figure 2-5: Mixed-Use Program - Alternative D



Chapter 2: Vision, Form, and Character

2.3 - Public Realm and Landscape Character

2.3 - Public Realm and Landscape Character



Aerial view of SR 126

Traditionally, in the ranch design, the entry drive is accented by a colonnade of native canopy or fruit trees along one or both sides of the drive accentuating the length of the drive and size of the property. Windbreaks strategically placed provide a sense of scale, shelter and emphasized the entry approach from great distances. Low fencing or mined cobble stones from the ranch grounds are sometimes employed to border the vast acreages of the ranch. Because many rural landscapes have been built at different times, with many different pieces, the character of the site should reflect the stitching together of the ranch history of the past with the present day uses.

2.3.1 - Landscape objectives:

- Emphasize the rural ranch character of the region.
- The landscape must define, unify and enhance the public realm – including the ceremonial entrance into the City of Santa Paula, SR 126, Hallock, entrance drives, creek green, parking lots and perimeter edges.
- The landscape must be sensitive to its environmental context and utilize plant species that reduce the need for supplemental irrigation water. Plant material must be compatible with an arid environment. Use a palette of native and drought tolerant naturalized ornamental plant species compatible with the natural vegetation of the region. Selected Plant species should have a Moderate, Low or Very Low water use rating as designated by the Water Use Classification of Landscape Species (WUCOLS) list for Southern California Coastal environments.

- Landscape must be in accordance with local and State Firescape guidelines, fire safe plant lists, and City of Santa Paula Resolution No. 3675, adopted March 20, 1989
- Existing drainage ways must be preserved and enhanced. The project site will be designed to maintain the natural storm water flows by promoting on site infiltration and reducing contaminants through biological filtration. The objective is to decrease runoff peak flow and volume by providing as many opportunities for bio-retention and on site infiltration. As a result the rate and volume of on site storm water infiltration is increased, and on site water cleansing/filtration is achieved.
- Implement a responsible storm water management system that treats runoff as an environmental resource and a design amenity. The landscape must cleanse and detain storm water on site by utilizing a combination of biofiltration, permeable paving and surface detention methods.



Native, organic landscape, sensitive to its environmental context

- a. Pervious Surfaces: Create pervious surfaces where practical. Alleys and on-street parking areas should be paved with pervious paving designed for rapid infiltration of storm water.
- b. Biofiltration: Cleanse the first flush from storm events by conveying rainfall to bioswales, rain gardens and other designated planting areas designed to filter water and reduce its velocity.

- c. Detention: Detain storm water in surface detention placed at low points in greens, parks, parkways and basins.
- d. Landscape amenity: Make visible storm water management systems aesthetically pleasing without the use of protective fencing. Conveyance and biofiltration methods must be integral to the landscape design and must add value to the experience of open space.
- e. Encourage the traditional rural ranch style curbless roadway condition adjacent to landscapes where storm water easily flows into the landscape.



Curbless roadway leads runoff water into a landscaped ditch



Curb cuts lead runoff water into landscaped rain gardens

2.3.2 - Landscape sustainability.

The goal for the overall landscape is to be an aesthetically pleasing solution where the resolution of designed elements creates a unified and harmonious rural ranch environment. Above all, the design integrates sustainable concepts and solutions that restore natural functions and processes. The overriding concept is to view the public right of way and urban runoff from pavement as an extension to the natural stream system and its ecology. The environmental implications of this concept are not restricted to the project site but impact the surrounding environment and the regional watershed.

Water efficient landscaping will be introduced, beginning with a soil/climate analysis to determine the most appropriate landscape design that includes the selection of indigenous and native-in-character, drought tolerant plants to reduce irrigation requirements. Lawn is recommended in areas of passive and active recreation and bioswales and discouraged in most other areas. Wherever lawn is used the selected species will be a deep-rooted variety with low watering requirements. Planted areas, except for lawn and seeded groundcover, will have a surface layer of specified recycled mulch to a depth of three inches. The mulch layer will assist in the retention of moisture and reduce watering requirements and will also minimize weed growth, reducing the need for chemical herbicide treatments.

Where irrigation is required, high efficiency irrigation technology with low-pressure applications such as drip, soaker hose, systems with rain shut-off devices and low volume spray systems will be used. The efficiency and uniformity of a low water flow rate reduces evaporation and runoff and encourages deep percolation. After the initial growth period of three to seven years (or once landscape has matured), irrigation will be limited.



Native, drought-tolerant plants that can handle an arid climate

Chapter 2: Vision, Form, and Character

2.4 Heritage Valley Highway Area

2.4 Heritage Valley Highway Area (Blocks 1 & 2)



A. Vision. The vision for the two blocks fronting SR 126 is a composite of a number of rural California roadside commercial centers and simple design elements that combine to project the image of the edge of a California small town, organized to support the programmatic needs of modern retailers.

B. Organization and Character of Blocks. These two blocks are organized and defined by an entry point along SR 126 and an entry point from Hallock Drive with the intent of generating a semi-rural edge along the highway with smaller, individual buildings along the highway and/or fronting the private streets.

C. Buildings and Architectural Character. Buildings in this area are the smallest within the plan area and separated by natural open space, courtyards, dining patios, and or small parking areas.

- **Architectural Character:** Buildings are relatively small “pad” buildings with simple gabled roof forms and simple or rustic materials recalling the local and regional traditions of agricultural buildings, roadside stands, “main street” retail buildings and highway commercial businesses. Buildings



Conceptual Plan of blocks and streets

are located along the highway edge or along the internal streets with their entries oriented to the internal private streets and parking areas, not to SR 126. Corner entries accessible from parking areas as well as internal streets are highly encouraged. This relationship between the highway frontage and the internal street frontage results in the need for these buildings to be carefully designed with “4-sided architecture” to ensure attractive views from the highway.

D. Landscape Character. Three general areas define the setting for buildings and activity:

- **Heritage Valley Highway Frontage:** A landscaped highway edge setback characterized by native California plantings in an informal, rural composition of drought tolerant ground covers, perennials. Large trees vertically define the highway edge while providing clear views of buildings below their canopies. A bike path set back from the roadway, midway between the highway curb and the buildings, with a low rustic wall or fence between pedestrians and highway traffic defines this edge of the Plan Area.
- **Creek and Greenbelt Edge:** A small unnamed creek runs through Block 2 and continues along the easterly edges of the plan area. Buildings and parking areas are set back substantially from the centerline of this creek, with naturalistic native landscaping within the creek corridor. Outdoor dining or rest areas may abut the creek corridor.
- **Open Space:** A simple rectangular parking area - shaded with large trees compatible in character with the buildings along the highway frontage - occupies the center of these blocks. Outdoor dining areas or other usable courts or gardens are sited between or adjacent to the buildings, designed with shade trees, fruit trees, and seating areas for outdoor dining in a comfortable setting similar to so many roadside stops that were common throughout the region into the 1960’s.

E. Land Use and Parking: The most likely program for this area are a mix of small, family-oriented, convenience-oriented restaurants with other possible business types including retail or service commercial businesses, or some office uses associated with an R&D program. The general prioritization of land uses for this area is summarized below:



Restaurant with simple massing and metal siding



Outdoor rest area adjacent to creek

	Incompatible	Low	High
Agricultural		•	
Automotive/Vehicle-Related		•	
Eating/Drinking			•
Medical		•	
Professional Service			•
Professional and Administrative			•
Retail			•
Service/Commercial			•
Industrial	•		

Parking is shared among the land uses and is provided between or behind buildings within the center of the blocks.

Chapter 2: Vision, Form, and Character

2.5 Central Area

2.5 Central Area (Blocks 3 & 4)

A. Vision. The vision for these two central blocks is that they be quite flexible in the buildings and uses they accommodate with the primary opportunities being a variety of retail and office, with limited industrial and service commercial activity.

B. Organization and Character of Blocks: As is the case for all blocks within the Plan Area, the design character of these blocks will be substantially defined by their edges, which include:

- **Hallock Drive:** It is important that buildings on this frontage relate in their siting with buildings on the southwest corner of Block 1 to define the entry into the site from Hallock Drive.
- **Internal Streets:** Three of the four block-faces of each of these blocks are defined by Streets A, B and C, which include sidewalks and street tree plantings that define these primary circulation routes for drivers, bicyclists and pedestrians.
- **Service Drive:** The southwesterly angled edge of Block 3 abuts the adjoining industrial uses, and provides a very useful “back” for the block, which is to be the location for major service and delivery access for this block.
- **Retail Drive:** If the Regional Retail program alternative is pursued, a retail drive provides retail frontages and addresses for a building or buildings within the westerly portion of the block. Such a Drive would most likely not be provided for the R&D / Light Industrial alternative.
- **Edge Drive:** An edge drive provides access along the edge of the site for either the Retail Center, the Mixed-Use, or the R&D / Light Industrial alternatives.
- **Creek Edge:** The easterly edge of Block 4 is defined by the southerly continuation of the small unnamed creek, which forms the boundary between the Plan Area and the Santa Paula/Fillmore Greenbelt. The character of that edge is substantially the same as that of the creek within Block 2, and is described in detail in Section 3.3 of Chapter 3, abutted by parking areas or an Edge Drive.



Conceptual Plan of blocks and streets



Retail buildings with simple, agrarian character



Light industrial/R&D buildings

C. Buildings and Architectural Character. Buildings in this area are at least as large as the buildings along the highway.

- **Additional ‘Pad’ Buildings:** Buildings are similar to those in Blocks 1 and 2, and/or parking lots for buildings located in other blocks. Block 4, in particular, might be developed with one or two buildings or none, to provide parking needed by larger buildings in adjoining blocks.
- **Retail and/or R&D and office buildings:** Most likely, within block 3, larger retail buildings would be developed along with parking areas to support those uses. Alternatively, moderately sized R&D buildings within blocks 3 and 4 with parking in front or behind could also be developed.

D. Landscape Character. Five streetscapes define the setting for buildings and activity; “Hallock Drive”, “Internal Streets”, “Retail Drives”, “Edge Drive”, and “Service Drives”. Each are defined in detail in section 3.4.6B - “Streetscape Landscaping.” With the exceptions of Service Drive, each is characterized by tree plantings on both sides, and a sidewalk on one side.

E. Land Use and Parking. The most likely programs for this area are primarily small to large format retail and restaurants as well as office or limited service commercial and light industrial associated with an R&D program. The general prioritization of land uses for this area is summarized below:

	Incompatible	Low	High
Agricultural		•	
Automotive/Vehicle-Related		•	
Eating/Drinking			•
Medical			•
Professional Service			•
Professional and Administrative			•
Retail			•
Service/Commercial			•
Light Industrial			•

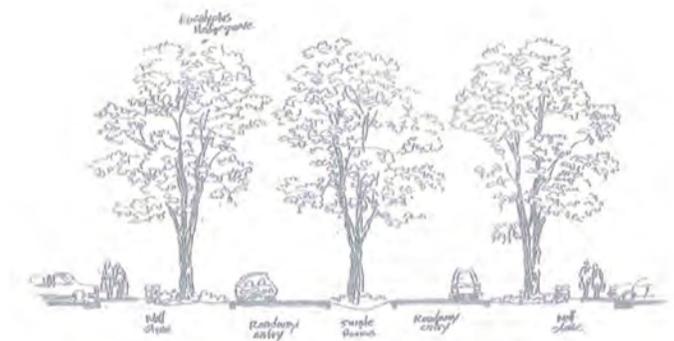
Parking is shared among the land uses and is provided between or behind buildings within the center of the blocks.



Alternative arrangement focusing on an R&D program



Illustration of Creek Edge, abutted by parking or an Edge Drive



Cross section through Street A between Blocks 1 and 2

Chapter 2: Vision, Form, and Character

2.6 River Edge Blocks

2.6 River Edge (Blocks 5 & 6)

A. Vision. The vision for these most southerly blocks is to accommodate the largest buildings in the plan area while maintaining connectivity with the northerly four blocks and the adjacent river edge. Two general alternatives address this vision as described below:

- **Regional Retail Alternative:** This program alternative would most likely organize a number of large retail users along the southerly edge of the Plan Area, with a Retail Drive terminating Street A. Buildings front the retail drive with a combination of shopfronts, galleries and forecourts for outdoor display and merchandising.
- **Employment District Alternative:** This program alternative would organize larger buildings along Street A for light industrial activity, or alternatively with smaller, office and R&D buildings.

B. Organization and Character of Blocks. As with Blocks 3 and 4, the character and organization of Blocks 5 and 6 will be substantially defined by their edge conditions and access drives, including:

- **Internal Streets:** Two of the four block-faces of each of these blocks are defined by Streets A and C, which include sidewalks and street tree plantings that define these primary circulation routes for drivers, bicyclists and pedestrians.
- **Service Drives:** The westerly edge of Block 5 and the southerly edges of Blocks 5 and 6 are served by Service Drives, which provide service and delivery access, as well as access to employee parking areas.
- **Retail Drive:** If the Regional Retail program alternative is pursued, a Retail Drive would provide frontages and addresses for retail buildings within the southerly portion of the block. Such a Drive would most likely not be provided for the Employment District alternative.
- **Edge Drive:** Such a drive would provide access along the edge of the site for either the Retail Center or Employment District program.



Conceptual Plan of blocks / streets for Regional Retail Alternative



Well designed frontages of "big box" retailers



Strong frontage design of an Anchor Retail Tenant

C. Buildings and Architectural Character. Buildings in this area are the largest in the plan area.

- Large retail buildings front streetscapes along Streets 'A', 'C', and retail drives with parking areas in front, behind or along side to support those uses.
- Alternatively, R&D and office buildings varying in size front streetscapes along Streets 'A', 'B', and 'C' with parking areas in front, behind or along side to support those uses.

D. Landscape Character. Four streetscapes define the setting for buildings and activity; "Internal Streets", "Retail Drive", "Edge Drive", and "Service Drive". Each are defined in detail in section 3.4.6B - "Streetscape Landscaping." With the exceptions of Service Drive, each of these is characterized by tree plantings on both sides, and a sidewalk on one side.



Conceptual Plan of blocks/streets for Employment District Alternative

E. Land Use and Parking: The most likely programs for this area are primarily large format retail and restaurants as well as office, limited service commercial and light industrial associated with an R&D program. The general prioritization of land uses for this area is summarized below:

	Incompatible	Low	High
Agricultural		•	
Automotive/Vehicle-Related		•	
Eating/Drinking		•	
Medical			•
Professional Service			•
Professional and Administrative			•
Retail			•
Service/Commercial			•
Light Industrial			•

Parking is shared among the land uses and is provided between or behind buildings within the center of the blocks.

Chapter 2: Vision, Form, and Character

2.7 River Edge Blocks

2.7 Gateway Triangle Site (Block 7)



A. Vision. The vision for this triangular parcel fronting SR 126 as the visitor enters Santa Paula is based on a traditional, small town California roadside commercial type. Simple one-story gabled buildings - evoking images of agricultural sheds or fruit stands - with rustically landscaped parking areas are the primary elements. The highway frontage is defined by a drainage swale that will be landscaped with native plants, river rock and a low open fence to resemble a dry creekbed, with a unique sign at the point of the site welcoming visitors to Santa Paula.

B. Site Organization. The illustrative plan shows a single building at the west end of the property, facing oncoming westbound highway traffic, with the parking area to its east. A similar additional building might also be located close to the east end of the triangle, providing a backdrop for the City gateway sign, and reinforcing the image of this side as a roadside commercial establishment. Access to the site is provided by Old Telegraph Road at the northwest corner.



Conceptual Plan of blocks / streets for Regional Retail Alternative

C. Buildings and Architectural Character. Buildings in this area are the smallest within the plan area and flanked by dining patios, outdoor merchandise display and rustically landscaped parking areas.

- **Architectural Character:** Buildings are small with simple gabled roof forms and simple or rustic materials recalling the local and regional traditions of agricultural buildings, roadside stands “and highway commercial businesses. The primary building - or potentially a pair of smaller buildings - is located at the west end of the side, facing to the east and toward the highway. A potential additional small building may be located near the east end of the site, facing motorists entering Santa Paula from the east. The appearance of these buildings from the highway, with the naturalistic rural landscape as their foreground, is the key design objective. These buildings are to be designed with “4-sided architecture”, except that service functions for the easterly buildings should be along their west face, toward the existing gas station..

D. Landscape Character. Three general areas define the setting for buildings and activity:

- **Heritage Valley Highway Frontage:** A landscaped highway edge setback characterized by native California plantings in an informal, rural composition of drought-tolerant ground covers, perennials. Large trees with high canopies - eucalyptus or a tree of similar habit - vertically define the highway edge while providing clear views of buildings below their canopies. No sidewalk is proposed across this frontage, as it leads only to the railroad tracks and the north edge of the rural highway to the east.
- **Open Space:** A simple triangular parking area - shaded with large trees compatible in character with the buildings along the highway frontage - occupies the center of this block. Outdoor dining areas or other usable courts or gardens may be sited or adjacent to the buildings, designed with shade trees, fruit trees, and seating areas for outdoor dining in a comfortable setting similar to so many roadside stops that were common throughout the region into the 1960’s.

E. Land Use and Parking: The most likely program for this area are small markets or restaurants, or potentially service commercial businesses. The general prioritization of land uses for this area is summarized to the right.



Simple rustic buildings compatible with the rural environment to the east



Outdoor displays of locally produced merchandise are recommended

	Incompatible	Low	High
Agricultural	•		
Automotive/Vehicle-Related	•		
Eating/Drinking		•	
Medical			•
Professional Service			•
Professional and Administrative			•
Retail			•
Service/Commercial			•
Light Industrial		•	

Parking is shared among the land uses and is provided between or behind buildings within the center of the blocks.

Chapter 3: East Gateway Code

3.1 Administration

3.1 Administration

3.1.1 Purpose

This chapter of the East Gateway Specific Plan is the ‘East Gateway Code’ and operates as an integrated set of standards to implement the East Gateway Specific Plan. The East Gateway Code is adopted to protect and promote the public health, safety, comfort, convenience, and general welfare of the community and implement the Santa Paula General Plan for the identified area in Figure 1 ‘East Gateway Code Boundaries.’

3.1.2 Authority

This chapter of the East Gateway Specific Plan is authorized by and prepared in accordance with the requirements of Government Code §§65450 through 65457) and Santa Paula Municipal Code Chapter 16.216.

3.1.3 Interface with SPMC

This chapter places many standards for the property within the East Gateway Code boundaries identified in Figure 1 while continuing to rely on certain regulations of the SPMC. Unless the East Gateway Code specifically provides differently, the SPMC governs. Specifically, the East Gateway Code will replace portions of the Santa Paula Development Code (Title XVI) as identified in Table 1:

TABLE 1: INTERFACE WITH SPMC	
Existing SPMC Section that applies to Plan Area	Replaced by East Gateway Code Section
16.05 ‘Definitions’	Certain definitions as identified in Appendix A
16.15.020 Table 15-1 (Land Use and Permit Requirements for the CH zone)	Table 2
16.15.030 Table 15-2 (Development Standards for the CH zone)	Tables 3A, 3B, 3D
16.21.020 Table 21-1 (Land Use and Permit Requirements for the C-LI zone)	Table 2
16.21.030 Table 21-2 (Development Standards for the C-LI zone)	Tables 3B, 3C
16.40.020 Access and Circulation	Figure 2
16.40.030 Boundary Walls	Table 4C
16.40.050 Clear Sight Triangle Area	Tables 3A - 3D Section ‘A’
16.44.020 Fences, Walls, Hedges: Maximum Height	Table 4C
16.46.040 Number of Parking Spaces Required)	Table 2
16.40.060 Shared Parking and Table 46-1 (Off-Street Parking Requirements)	Section 3.2.8B
16.46.080 (Driveway and Access Standards--Non-Residential Uses)	Tables 3A - 3D Section ‘C’
16.46.130 Required Screening	Section 3.4.7C
16.48 Sign Regulations	Section 3.5

Figure 3-1: Code Boundaries



Chapter 3: East Gateway Code

3.1 Administration

3.1.4 Applicability

The East Gateway Code applies to any of the following within the boundaries identified in Figure 1:

- a. land use activity,
- b. new buildings and signage,
- c. modifications/additions to existing buildings and signage,
- d. subdivision of land or a building,
- e. improvements to a site

All such actions must comply with the East Gateway Code as follows:

A. Requirements for new structures or land uses, or changes to structures or land uses. The City cannot issue a permit unless the proposed project complies with all applicable provisions of the East Gateway Code, including the applicable Required Findings.

B. Legal Lot. The site of proposed land use, building, sign, subdivision, improvement or modification subject to the East Gateway Code must be on a lot(s) legally created in compliance with the Subdivision Map Act (Government Code §§66410, *et seq.*) and the applicable standards for lot size.

C. Minimum requirements. The regulations in the East Gateway Code are minimum requirements for the protection and promotion of the public health, safety, and general welfare. When this Code provides for discretion on the part of a City official or body, that discretion may be exercised to impose conditions on the approval of any project proposed in the area subject to this Code.

D. Development and Design Standards, Conditions of Approval. Each structure, sign or site, addition(s) or modification(s) to a structure(s), sign, or site and land use(s) must comply with all applicable standards of Section 3.2 for

land use and parking standards; Section 3.3 for standards on building and parking placement, building height, frontage, and encroachments into required setbacks; Section 3.4 for standards on building design, lighting, services and landscape; Section 3.5 for signage standards; and any applicable provisions of the SPMC.

E. Allowable Land Uses. Allowed land uses are either allowed by right or by Conditional Use Permit as identified in Table 2.

F. Parking and Access. Required parking must be provided per Table 2. Required parking must be accessible per the standards identified in Section 3.3.

G. Design Review Process. All applications for new buildings and signs or modifications to buildings and signs are subject to review by the City pursuant to the SPMC.

3.1.5 Organization and Use of East Gateway Code

The East Gateway Code consists of the following Sections:

A. Administration. Section 3.1 establishes the purpose and the applicability of certain standards to property within the boundaries identified in Figure 1.

B. Regulating Plan and Zoning Standards. Section 3.2 changes the zoning designation for all property within the East Gateway Code boundaries identified in Figure 1. Specifically, property within the CH (Highway Commercial) and C-LI (Commercial-Light Industrial) zones will be regulated by the East Gateway Specific Plan (EGSP) zoning designation. The EGSP zone is aimed at enabling the wide range of commercial, office and light industrial uses envisioned in the East Gateway Specific Plan and allocates the appropriate land uses and parking requirements in Table 2 to carry out the vision of the Specific Plan. The rest of the standards that implement the EGSP are provided in Sections 3.3, 3.4 and 3.5 of the East Gateway Code. Figure 2 identifies a new zoning designation for all property within Figure 1,

which is incorporated into the City's Official Zoning Map (SPMC §16.07.020).

C. Development Standards. Section 3.3 identifies the requirements for where buildings and parking are allowed on the site, how each building shapes the streetscape, maximum building height, and allowed encroachments into setbacks / rights-of-way. These standards are identified in Tables 3A - 3D in response to four different locations on the site:

- 1) blocks 1 and 2;
- 2) blocks 3 and 4;
- 3) blocks 5 and 6; and
- 4) block 7.

D. General Standards. Section 3.4 identifies standards that apply to all development such as building and facade massing standards and guidelines, landscaping requirements, and requirements for the location of trash enclosures, utilities, services, lighting. These standards are identified in Tables 4A - 4C.

E. Signage Standards. Section 3.5 identifies the allowed signage as it corresponds to the variety of building and streetscape scenarios allowed in Section 3.3. These standards are identified in Tables 5A - 5G.

F. Definitions. Appendix A defines the terms and phrases used in the East Gateway Code.

3.1.6 Permit Approval Process

All required permits/approvals must be obtained before the proposed use, and any structures related to the proposed use, are constructed, otherwise established or put into operation.

Unless specified otherwise, the East Gateway Code will be

administered and enforced by the City of Santa Paula Planning Department, Planning Commission, and City Council per SPMC Chapter 16.200. Applications must be processed and approved by the review body per SPMC Table 200-1 if the City determines the application to be consistent with the applicable requirements and findings of the East Gateway Code.

3.1.7 Minor Modifications

As identified in SPMC Chapter 16.222 "Minor Modifications," particular standards of the East Gateway Code may be adjusted subject to the applicant providing the necessary information for the Planning Director to make an informed decision in granting or denying the request for modification. The Planning Director may refer the application to the Planning Commission. The standards in Chapter 3 of the East Gateway Specific Plan have been prepared per the expectations and vision identified in Chapter 2. In addition, flexibility as to processing of applications has been integrated into the standards and processing procedures of Chapter 3. Therefore, applications that exceed the limits of SPMC Chapter 16.222 are considered inconsistent with the East Gateway Code and therefore, require a variance per SPMC §16.220.

3.1.8 Effect on Existing Development and Land Uses

Buildings and land uses legally existing as of the adoption of the East Gateway Code may continue until such time that the building or land use activity does not comply with the requirements of SPMC § 16.110 (Nonconformities).

3.1.9 Amendments to the East Gateway Code

Amendments to the East Gateway Code must be processed per the requirements of SPMC §16.216.080.

Chapter 3: East Gateway Code

3.2 Regulating Plan and Zoning Standards

3.2 Regulating Plan and Zoning Standards

3.2.1 Purpose

This section establishes the development standards that will implement the various intentions of the East Gateway Specific Plan. The zoning designation for the entire Plan area is East Gateway Specific Plan, and all future development of infrastructure, buildings, landscape and other site improvements must be found to be consistent with the standards of this Chapter, and with the design intent of Chapter 2.

3.2.2 Regulating Plan

This section establishes a Regulating Plan, which organizes the Plan Area into 7 Blocks, which are defined by the location and configuration of several Private Streets and Drives, and specific and slightly differentiated development standards for buildings, landscape and other site improvements are assigned to those Blocks and Streets and Drives.

3.2.3 Zoning

The East Gateway Specific Plan (EGSP) replaces the CH and C-LI zones currently applied to the Plan Area. For property within the East Gateway Code boundaries but not within the City of Santa Paula incorporated boundaries, the current Ventura County zoning will apply until the owner pursues a change in land use activity, improvements, or development. At such time, the property will be required to comply with the East Gateway Specific Plan. The East Gateway Specific Plan designation is applied to these unique properties that face and connect to the Heritage Valley Highway (SR 126) for the purpose of providing community and regional retail uses as well as office and light industrial activity.

Buildings are up to two stories in height, are located near or at the sidewalks of the Private Streets and Drives, and vary in size from small freestanding buildings to large freestanding or attached buildings. Land uses range from convenience to regional and warehouse retail to office to small-scale light industry and automotive-related repair. Parking is shared throughout the area and is either in front, along the sides of buildings, or behind buildings depending upon the actual location on the site.

In order to implement the intended physical character that

accommodates the range of land uses allowed by the EGSP the standards are applied to the Plan Area in terms of its Blocks and its Streets and Drives, as shown in Figure 2.

- Blocks 1, 2 and 7: These blocks, and the street frontage that abut them, are intended to provide for a range of smaller commercial buildings, organized around small patios or courts and parking lots, with a small town highway commercial character.
- Blocks 3 and 4: These blocks may accommodate similar small commercial buildings, larger retail buildings, or office/R&D buildings of various sizes.
- Blocks 5 and 6: These blocks may accommodate larger retail buildings, larger industrial buildings, or potentially groupings of smaller employment-related buildings if the market supported such development.

Key to Figure 2

-  East Gateway Specific Plan Area (see Table 2 for allowed land uses)
-  Reference to block
-  Potential Building (actual size and location subject to design and City approval per East Gateway Code)
-  Minimum dimension range 'i' - 250-300 ft.
-  Minimum dimension range 'ii' - 300-350 ft. [1]
- Circulation: (see Table 3-E for details)**
-  Entry Access Type A - Right-in, Right-out only
-  Entry Access Type B - Full Access
-  Heritage Valley Highway Frontage
-  Private Street Type 'A'
-  Private Street Type 'B'
-  Private Street Type 'C'
-  Drive Type 'A'
-  Drive Type 'B'
-  Drive Type 'C' (service)

[1] n/a for Regional Retail Center Scheme 'B'

Chapter 3: East Gateway Code

3.2 Regulating Plan and Zoning Standards

3.2.4 Development Standards

The East Gateway Specific Plan designation is implemented through standards outlined below in support of the vision presented in Chapter 2:

1. Table 2: Allowed Land Uses and Required Parking
2. Tables 3A - 3D: Building and Parking Placement, Building Height, Allowed encroachments into required yards, Property Frontage and Landscaping.
3. Figure 3: Landscape Regulating Plan
4. Tables 4A - 4C: Building Massing and Facades, Landscape and Plant Materials, and Lighting, Service, Utilities
5. Tables 5A - 5G: Allowed Signage Types

3.2.5 Required Findings

Applications involving Section 3.2 of the East Gateway Code must be consistent with the applicable findings below in order to be approved:

1. "The proposed application..."
 - a. Generates or maintains the intended physical character based on the vision, policies and standards of the East Gateway Specific Plan;
 - b. Is in compliance with the applicable requirements of Figure 2 and Development Standards of this Chapter;
 - c. Generates or maintains compatibility between adjacent and surrounding conforming land use types;
 - d. Will achieve land use compatibility upon including the necessary requirements in Table 2;
 - e. Generates or maintains the intended streetscape as envisioned in the East Area 2 Specific Plan;
 - f. Generates or maintains compatible adjacencies of building location and scale between new buildings and

3.2.6 Land Use Standards

The land use standards in this Section identify the land use types allowed by the City in each zone established by the Regulating Plan and determine the type of City approval required for each land use type.

3.2.7 Allowed Land Uses and Requirements

A. Allowed Land Use. A parcel or building within the code boundaries can be occupied by only the land uses allowed by Table 2. Definitions of listed use types are in the SPMC except where a specific type is defined by Appendix A (Definitions) of the East Gateway Code. If a land use type is not defined in this Section, or in the SPMC, the Planning Director can determine the correct definition. Terms not defined in the East Gateway Code or in the SPMC may be defined by the common English dictionary.

1. Establishment of an allowed land use.

- a. Any one or more land uses identified by Table 2 as being allowed may be established on any parcel within the code boundaries, subject to the planning permit requirement listed in the table, and in compliance with all applicable requirements of these Development Standards.
- b. If a parcel is proposed for development with two or more of the land uses listed in the tables at the same time, the overall project is subject to the highest permit level required by the tables for any individual use. For example, a new multi-use building proposed with a permitted use in one area of the site and a use requiring Conditional Use Permit approval on another area of the site would require Conditional Use Permit approval for the entire project.

2. Land Use not listed. A land use not listed in Table 2, that is determined by the Planning Director to not be included in Appendix A (Definitions) under the definition of a listed land use, is prohibited within

the code boundaries, except as otherwise provided in Subsection A(3) 'Similar and Compatible Land Use may be allowed'.

3. Similar and compatible land use may be allowed.

The Planning Director may determine that a proposed use not listed in Table 2 is allowable as follows:

a. Required findings. A determination that a proposed use is similar to, and compatible with a listed use and may be allowed, requires that the Director first make all of the following findings:

1. The characteristics of, and activities associated with the use are similar to one or more of the listed uses, and will not produce greater impacts than the uses listed for the zone;
2. The use is consistent with the purpose of the zone;
3. The use is consistent with the General Plan and the East Gateway Specific Plan;
4. The use will be compatible with the other uses allowed in the zone; and

b. A determination of "similar use" and the findings supporting the determination must be in writing.

4. Permit requirements and development standards.

When the Planning Director determines that a proposed, but unlisted, use is similar to a listed use, the proposed use will be treated in the same manner as the listed use in determining where it is allowed, what permits are required, and what other standards and requirements of these Development Standards apply.

B. Permit requirements. Table 2 provides for land uses that are:

1. Permitted subject to compliance with all applicable provisions of Table 2 and specific requirements in

Section 3.2.6. These are shown as "P" uses in the table;

2. Permitted subject to the approval of a Temporary Use Permit in compliance with the requirements of Section 3.2.8C. These are shown as "TUP" uses in the table;
3. Allowed subject to the approval of a Conditional Use Permit, and shown as "CUP" uses in the table;
4. Prohibited and shown as a "X" in the table.

C. Additional City approval requirements. Any land use identified as allowed by Table 2 may require other City permits, licenses, and approvals, including without limitations a building permit.

Chapter 3: East Gateway Code

3.2 Regulating Plan and Zoning Standards

TABLE 2: LAND USE AND PARKING STANDARDS

Land Use Type ¹	Permit Required by East Gateway Specific Plan Designation				Required Parking ⁴ min spaces per 1,000 sq ft	Definition
	Block					
	1-2	3-4	5-6	7		

AGRICULTURAL USE TYPES

Agricultural Production	p min 10 acres		p min 10 acres	P	0	16.05.020
Fruit/Agriculture Stand(s)	TUP ⁵	X	X	P	2	Appendix A

AUTOMOTIVE / VEHICLE-RELATED USE TYPES

Parts, sales, without installation	P	P	P	X	4	Appendix A
Parts, sales with installation	X	p ⁸	p ⁸	X	5 + 1 per service bay	
Repair (minor) - lube/tune, window tinting	X	p ⁸	p ⁸	X		
Service Station (including car wash)	X	CUP ⁸ (block 3 only)	X	X		16.05.020, 040
Repair (major) - bodywork, painting, engine	X	CUP ⁸		X		
Vehicle leasing/rental	P	P	P	X	4	Appendix A
Vehicle Sales	X	X	X	X	n.a.	
Vehicle storage, including RV's and Boats	X	X	X	X	n.a.	

EATING / DRINKING USE TYPES

Cafe, Delicatessen (no alcoholic beverage sales)	P	P	P	P	5	16.05.190
Restaurant (may include drive-through)	P	p (block 3 only)	X	X	5	
Restaurant (without drive-through), incl 'Fast-Food', 'Sit Down'	P	P	P	p max 2500 sf	8	
Sale of Alcoholic Beverages (part of eating establishment)	CUP ²	CUP ²	CUP ²	CUP ²	0	
Sidewalk dining (may be in private street right-of-way)	p ^{2,6}	p ^{2,6}	p ^{2,6}	p ^{2,6}	0	Appendix A

ENTERTAINMENT / RECREATION USE TYPES

Batting cage(s), driving range, similar outdoor facility	X	CUP	CUP	X	5	16.05.020
Bowling Alley	X	CUP	CUP	X	5	
Cultural Center	P	P	P	P	4	
Health/Athletic Club	CUP	CUP	CUP	X	5	
Indoor Recreation Center	P	CUP	CUP	X	5	
Special Event	TUP ⁵	TUP ⁵	TUP ⁵	TUP ⁵	per City Approval	Appendix A

Key to Zone Symbols

EGSP	East Gateway Specific Plan
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Key to Permit Types

- P Permitted Use
- TUP Temporary Use Permit
- CUP Conditional Use Permit required
- X Use not allowed

Requirements:

1. Definitions of listed use types are in the SPMC, Chapter 16.05, except where a specific type is defined by Appendix A (Definitions).
2. Sales of alcoholic beverages allowed per requirements of SPMC Chapter 16.58
3. Adult or child care allowed per requirements of SPMC Chapter 16.60
4. Per Requirements for Shared Parking in Section 3.2.8B

TABLE 2: LAND USE AND PARKING STANDARDS

Land Use Type ¹	Permit Required by East Gateway Specific Plan Designation				Required Parking ⁴ min spaces per 1,000 sq ft	Definition
	Block					
	1-2	3-4	5-6	7		

MEDICAL USE TYPES

Blood bank	X	CUP	CUP	X	3	Appendix A
Medical Laboratory	X	CUP	CUP	X	3	
Medical Office	P	P	P	X	4	
Urgent Care Facility	CUP	CUP	CUP	X	4	

PERSONAL SERVICE / FINANCIAL USE TYPES

ATM	P ¹¹	P ¹¹	P ¹¹	P ¹¹	0	16.05.020
Bank, Savings and Loan, Credit Union	P	P	P	X	3	16.05.070
Barber/Beauty/Nail/Salon	P	P	P	X	5	Appendix A
Child or Adult Day Care	CUP ³	CUP ³	CUP ³	X	1 per 5 guests	16.05.050
Day Care center - employer-sponsored	P ³	P ³	P ³	X	1 per 5 children	
Studio - Dance/Music School/Exercise	P	P	P	X	5	
Dry Cleaner (without on-site cleaning facility)	P	P	P	X	3	
Dry Cleaner (with on-site cleaning facility)	X	X	CUP	X	3	
Laundromat	P	P	P	X	1 per 3 washers	16.05.130
Pet Grooming	P	P	P	X	3	Appendix A
Repair: leather, luggage, shoes, electronics, small appliances	P	P	P	X	3	16.05.170

PROFESSIONAL AND ADMINISTRATIVE USE TYPES

Medical, Dental, Optometry, Chiropractic	X	P	P	X	5	16.05.160
Business, Professional, Research	X	P	P	X	4	16.05.160
Animal Hospital, Veterinary Clinic	CUP	P	P	X	4	16.05.230
Kennel (within a building)	CUP	P	P	X	4	16.05.120

5. Allowed per Section 3.2.8C Requirements for TUP's
6. Allowed per Section 3.2.8D - Requirements for Sidewalk Dining
7. Allowed per Section 3.2.8E Requirements for Sidewalk Sales / Display of Merchandise
8. Allowed per Section 3.2.8G Requirements for Vehicle Service / Repair businesses
9. Allowed per Section 3.2.8H Requirements for Farmer's Market activities
10. Or as otherwise approved by the City under a shared parking agreement for adjacent uses demonstrated to have different periods of peak parking demand
11. Facilities must be designed integrally with their associated building or site when a building is not involved so as not to detract from the intended visual character of the Specific Plan area.

Chapter 3: East Gateway Code

3.2 Regulating Plan and Zoning Standards

TABLE 2: LAND USE AND PARKING STANDARDS

Land Use Type ¹	Permit Required by East Gateway Specific Plan				Required Parking ⁴ min spaces per 1,000 sq ft	Definition
	Block					
	1-2	3-4	5-6	7		

RETAIL / COMMERCIAL USE TYPES

Antique sales	P	P	X	X	3	16.05.020
Art gallery, studio	P	P	P		1	
Auction (within a building, excluding livestock)	X	X	X	X	n.a.	16.05.190
Bakery (can include on-premises baking)	P	P	P	X	4	
Books	P	P	P	X	4	
Clothing / Apparel	P	P	P	X	4	
Candy	P	P	P	X	4	
Convenience / mini-market (up to 5,000 sq ft floor area)	P	P	X	X	4	
Fabric, crafts	P	P	P	X	4	
Farmer's Market ⁹	P	P	P	P	0	
Flowers	P	P	P	P	4	
Furniture, furnishings, appliances	P	P	P	X	4	
Groceries / market ²	P	P	P	X	5	
Hardware	X	P	P	X	4	
Home Improvement, lumber, garden	X	P	P	X	4	
Jewelry	P	P	P	X	4	
Music, instruments	P	P	P	X	4	
Nursery/Garden supply	X	P	P	X	4	
Office supplies, equipment	P	P	P	X	4	
Outdoor Sales	P ⁷	P ⁷	P ⁷	P ⁷	0	
Pets, Supplies	P	P	P	X	4	
Pharmacy	X	P	P	X	4	
Plumbing supplies, equipment	X	P	P	X	4	
Warehouse retail	X	P	P	X	5	

SERVICE COMMERCIAL USE TYPES

Catering	P	P	P	X	2	Appendix A
Cleaning / Janitorial	P	P	P	X	2	
Copy center / Postal center	P	P	P	X	3	
Equipment rental, sales, service	P	P	P	X	4	
Interior Design	P	P	P	X	3	
Laboratory (medical, dental)	X	P	P	X	4	
Photography shop/studio, film processing	P	P	P	X	3	
Painting, supplies	X	P	P	X	3	
Publishing	X	P	P	X	3	
Travel / Tourism agency	P	P	P	P	3	

Key to Zone Symbols

EGSP	East Gateway Specific Plan
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Key to Permit Types

P	Permitted Use
TUP	Temporary Use Permit
CUP	Conditional Use Permit required
X	Use not allowed

Requirements:

1. Definitions of listed use types are in the SPMC, Chapter 16.05, except where a specific type is defined by Appendix A (Definitions).
2. Sales of alcoholic beverages allowed per requirements of SPMC Chapter 16.58
3. Adult or child care allowed per requirements of SPMC Chapter 16.60
4. Per Requirements for Shared Parking in Section 3.2.8B

TABLE 2: LAND USE AND PARKING STANDARDS

Land Use Type ¹	Permit Required by East Gateway Specific Plan				Required Parking ⁴	Definition
	Block				min spaces per 1,000 sq ft	
	1-2	3-4	5-6	7		

INDUSTRIAL USE TYPES

Assembly (small scale - electronics, small appliances)	X	P	P	X	2	16.05.020
Distribution (low intensity, local distribution)	X	CUP	CUP	X	2	
Industrial Vehicle Equipment Rental, Sales, Service	X	CUP	CUP	X	2	Appendix A
Manufacturing (small scale, electronics, small appliances)	X	CUP	CUP	X	2	16.05.140
Motion Picture Studio	X	CUP	CUP	X	2	Appendix A
Office as accessory to primary industrial use	P	P	P	X	3	
Welding / Repair (not incidental to other repair)	X	X	X	X	n.a.	16.05.140

OTHER USE TYPES

Antenna: satellite or dish ¹¹	P	P	P	P	0	16.05.020
Antenna: wireless communications ¹¹	P	P	P	P	0	
Assembly hall	X	P	P	X	20 (seating area only) ¹⁰	Appendix A
Carnival	X	X	X	X	n.a.	
Club, lodge, meeting hall	X	P	P	X	10 (assembly area only) ¹⁰	16.05.060
Educational institution	X	P	P	P	5	16.05.150
News rack	P	P	P	P	0	Appendix A
Telecommunications Facilities ¹¹	P	P	P	P	0	16.05.200
Trade / Technical School	X	P	P	X	10 (classroom area only) ¹⁰	16.05.230
Utility Distribution Station	X	CUP	CUP	X	1	

5. Allowed per Section 3.2.8C Requirements for TUP's
6. Allowed per Section 3.2.8D - Requirements for Sidewalk Dining
7. Allowed per Section 3.2.8E Requirements for Sidewalk Sales / Display of Merchandise
8. Allowed per Section 3.2.8G Requirements for Vehicle Service / Repair businesses
9. Allowed per Section 3.2.8H Requirements for Farmer's Market activities

10. Or as otherwise approved by the City under a shared parking agreement for adjacent uses demonstrated to have different periods of peak parking demand
11. Facilities must be designed integrally with their associated building or site when a building is not involved so as not to detract from the intended visual character of the Specific Plan area.

Chapter 3: East Gateway Code

3.2 Regulating Plan and Zoning Standards

3.2.8 Requirements for Specific Land Uses

The pattern of compatible land use activity is important to the City's objectives for functional and appealing buildings and streetscapes. Therefore, the following requirements are aimed at maintaining well-functioning and attractive streetscapes within the East Gateway Specific Plan. As required by Table 2, property must comply with the following requirements, as applicable:

- A. General Requirements.
- B. Requirements for Shared Parking
- C. Requirements for Temporary Use Permits
- D. Requirements for Sidewalk Dining
- E. Requirements for Sidewalk Sales/Display of Merchandise
- F. Requirements for Outdoor Storage
- G. Requirements for Vehicle Service Businesses
- H. Requirements for Farmer's Markets

3.2.8A General Requirements

1. All business activities must be conducted and located within an enclosed building, except that the following business activities, as permitted in Table 2, may be conducted outside of an enclosed building:
 - i. Plant nurseries
 - ii. Flower Stands
 - iii. Temporary recreational or entertainment uses
 - iv. Sidewalk Dining
 - v. Outdoor storage, assembly
 - vi. Temporary Sales, events
2. There cannot be any manufacturing, processing, compounding, assembling or treatment of any material or product other than that which is clearly incidental to a particular retail enterprise, and where such goods are sold

on the premises.

3. All permitted activity must be conducted in such a manner as not to have a detrimental effect on permitted adjacent uses by reason of refuse matter, noise, light, vibration, or lack of proper maintenance of grounds or buildings.

3.2.8B Shared Parking

The East Gateway Specific Plan area is intended to generate and maintain a particular physical environment and range of land use activity through a shared parking approach. The intent is to allow for each property to generate building area and land use activity while grouping the parking facilities throughout the plan area. This approach is for the dual purpose of making efficient use of parking facilities and to encourage walking between businesses and destinations by relieving individual properties of providing potentially duplicative parking throughout the plan area.

Individual businesses may choose to either provide parking on their individual building site or to share the available parking spaces within the code boundaries. Table 2 identifies the required parking for each allowed land use in the plan area. Compliance with the aggregate amount of required parking by all of the individual businesses in the plan area must be monitored by the City for land use compatibility. The City must determine the method(s) and level of information necessary to monitor parking.

3.2.8C Temporary Use Permits

Temporary Use Permits allow short term activities that might not meet the normal development or land use standards of the applicable zone, but may otherwise be acceptable because of their temporary, non-permanent, or seasonal nature. Temporary land use activities are allowed subject to the following requirements, as applicable:

1. **Exempt temporary uses.** The following minor and limited duration temporary uses are exempt from the requirement for a Temporary Use Permit. Uses that do not fall within the categories defined below must comply with section 3.2.8C2 'Allowed Temporary Uses', below.

a. Construction Yards – On-Site

- i. On-site contractors' construction/storage yard(s), in conjunction with an approved construction project on the same parcel.
- ii. The construction yard must be removed immediately upon completion of the construction project, or the expiration of the companion Building Permit, authorizing the construction project, whichever first occurs.

b. Emergency facilities. Emergency public health and safety needs/land use activities, as determined by the City.

2. Allowed Temporary Uses.

In addition to the land uses identified in Table 2 as requiring a Temporary Use Permit, the following activities also require a Temporary Use Permit.

a. Contractors' construction yards – off-site. The temporary use of a site for an off-site contractor's construction/storage yard(s). The permit may be effective for up to 12 months, or the expiration of the companion Building Permit, authorizing the construction project, whichever first occurs.

b. Events.

- i. Entertainment events. Arts and crafts exhibits, festivals, food events, other similar events as determined by the Planning Director for a period not to exceed 10 days twice per year, separated by at least 30 days between uses. A Special Event Permit must be obtained from the City Community Services Department before conducting activities occurring on or within the public rights-of-way or other publicly owned property, in compliance with the Santa Paula Municipal Code.
- ii. Outdoor sales displays / events. Outdoor sales are allowed throughout the year when conducted by a retail business holding a valid Santa Paula Business License and when conducted in compliance with Section 3.2.8E (Outdoor Sales / Display of Merchandise). For purposes of this Subsection an outdoor sales display cannot be no longer than three consecutive days in duration.
- iii. Seasonal sales events. Seasonal sales (Halloween pumpkin sales and Christmas tree sale lots) only by

businesses holding a valid Santa Paula Business License not to exceed 40 days for pumpkin and tree sales. All other seasonal sales cannot exceed four events per year with a maximum of 30 days for each event.

c. Storage of structures or equipment. Temporary storage structures not to exceed 30 days.

3. Conditions of Approval.

In approving a Temporary Use Permit application, the Planning Director may impose conditions that are deemed reasonable and necessary to ensure that the permit would be in full compliance with the findings required by Section 3.2.5 'Findings'.

4. Requirements and conditions from other City departments.

Other City departments (e.g. Public Works, Fire, and Police) may also recommend conditions for incorporation into the approved Temporary Use Permit as determined by the City.

5. Appropriate conditions.

These conditions may address any pertinent factors affecting the operation of the temporary event, or use, and may include the following:

- a. Fixed period of time. Unless otherwise stated in the permit, a provision for a fixed period of time not to exceed 30 days for a temporary use not occupying a structure, including promotional activities, or 12 months for all other temporary uses or structures, or for a shorter period of time as determined appropriate by the Director;
- b. Operating hours and days. Regulation of operating hours and days, including limitation of the duration of the temporary use, as specified on the Temporary Use Permit;
- c. Temporary pedestrian and vehicular circulation. Provision for adequate temporary pedestrian and vehicular circulation, parking facilities (including vehicular ingress and egress), and public transportation, if applicable;
- d. Regulation of nuisance factors. Regulation of nuisance factors including prevention of glare or direct illumination on adjacent parcels, dirt, dust, gases, heat, noise, odors, smoke, trash, and vibration;

Chapter 3: East Gateway Code

3.2 Regulating Plan and Zoning Standards

- e. Regulation of temporary structures. Regulation of temporary structures and facilities, including placement, height and size, location of equipment and open spaces, including buffer areas and other yards;
- f. Sanitary and medical facilities. Provision for sanitary and medical facilities, as appropriate;
- g. Waste collection, recycling, and/or disposal. Provision for solid, hazardous, and toxic waste collection, recycling, and/or disposal;
- h. Police/security and safety measures. Provision for police/security and safety measures, as appropriate;
- i. Signs. Regulation of signs;
- j. Performance bond or other security. Submission of a performance bond or other security measures and satisfactory to the Director, to ensure that any temporary facilities or structures used will be removed from the site within a reasonable time following the event and that the property will be restored to its former condition, or better, as determined by the Director, in compliance with subsection 3.2.8C7 'Condition of Site Following Temporary Use';
- k. Compliance with applicable provisions. A requirement that the approval of the requested Temporary Use Permit is contingent upon compliance with applicable provisions of the SPMC and the successful approval of any/all required permits from any other department or governing agency; and
- l. Other conditions. Other conditions that would ensure that the operation of the proposed temporary use would be conducted in an orderly and efficient manner, and in full compliance with the purpose of this Section.

6. Applicant's agreement. The applicant agrees in writing to comply with any and all of the conditions imposed by the review authority in the approval of the Temporary Use Permit.

7. Condition of Site Following Temporary Use. Each site occupied by a temporary use must be cleaned of debris, litter, or any other evidence of the temporary use upon completion or removal of the use, and must continue to be used in compliance with the East Gateway Code.

3.2.8D Sidewalk Dining

1. As allowed in Table 2, sidewalk dining may occur within a sidewalk or open space in accordance with the Santa Paula Municipal Code.
2. A permit may be issued, in addition to requirements of the SPMC, when the application complies with the following requirements:
 - a. furniture such as tables, chairs, umbrellas and portable heaters are allowed to be placed within the sidewalk provided that the furniture is:
 - i. located adjacent to the building or near the curb leaving the necessary passageway for pedestrians;
 - ii. clear of required ADA access;
 - iii. maintained in a manner that does not become detrimental to the function and appearance of the sidewalk or present potential safety hazards;
 - b. If alcohol is served, the area where alcohol is to be consumed must be delineated by decorative stanchions and/or potted plants or other such approved method;
 - c. The area along the sidewalk cannot be enclosed with any feature taller than 4 feet except for landscaping which cannot obscure views into the sidewalk dining area;
 - e. The area must be kept free of debris or other such trash.

3.2.8E Sidewalk Sales / Display of Merchandise

1. Outdoor display of merchandise for sale must be conducted in a manner that does not create any conditions that are detrimental to the appearance of the premises or surrounding properties or in any other manner is detrimental to the public health, safety, welfare or causes a public nuisance.
2. Displayed merchandise cannot be located within 4 feet of a paved travel lane, parking aisles, required parking spaces, landscaped areas, or within required fire or handicapped access ways.
3. Displayed merchandise cannot impede the clear accessible path along the sidewalk and entrance to the business and must be at least 4 feet from the adjacent curb;

4. Displayed products are limited to the primary merchandise sold by the principal business that occupies the building. Merchandise cannot be displayed for sale on an undeveloped or vacant site;
5. In multi-tenant buildings, the displayed merchandise from the multiple tenants displaying merchandise is limited to the area directly in front of the multi-tenant building displaying the merchandise.
6. Displayed merchandise must be removed each day when the business closes.
7. The property owner is responsible for the collection of trash and any debris caused by the display of merchandise in front of the building.

3.2.8F Outdoor Storage

1. All outdoor storage areas must be completely screened from both public view and adjacent properties to ensure that outdoor storage does not create any conditions that are detrimental to the appearance of the premises or surrounding properties or in any other manner that is detrimental to the public health, safety, welfare or causes a public nuisance.
2. The stored materials are limited to those normally associated with the principal use on the site as allowed by Table 2.
3. All walls or fences used for screening purposes must be between 5 and 8 feet tall with the height determined by the height of the materials to be screened.
4. Storage materials or equipment cannot exceed the height of the wall or fence enclosing the materials or equipment.
5. All screening materials must be installed and finalized before the commencement of storage on-site.
6. All walls or fences used for screening purposes must be consistent with the architectural style and materials of the building to which they are attached or serving. When walls are immediately adjacent to the building, the wall must be designed as an extension of the building facade in terms of materials and finishes. The wall or fence must be designed to achieve visual compatibility with adjacent buildings through the use and application of materials, finishes and colors.

3.2.8G Vehicle Service Business

1. Automotive repair establishments must comply with the following, as applicable to allow the activity while maintaining land use compatibility with adjacent buildings and land uses:
 - a. All repair activities and storage of customers' vehicles must be entirely enclosed within a building or behind freestanding walls at least 8 feet tall, subject to the applicable requirements for frontage in Section 3.3.
 - b. Off-street parking must be provided so that all vehicles under repair are within a building on the property where the repair activity occurs or behind a wall at least 8 feet tall.
 - c. All exterior building walls must comply with the applicable frontage requirements.

3.2.8H Farmer's Markets

1. Farmer's Markets, temporary. Temporary or seasonal sales of agricultural products such as vegetables, fruits and other related items are allowed per a Temporary Use Permit in the applicable zone per the following:
 - a. Agricultural products must comprise at least seventy-five percent (75%) of the retail space available.
 - b. Sales may occur in parking areas subject to City review and approval.
 - c. Sales may only occur in temporary displays intended to be installed and removed within a 24-hour period unless otherwise approved by the City.
 - d. The sale of alcoholic products is limited to wine and beer only, by the bottle, not to be consumed on-site, and is subject to securing a conditional use permit for the sale of alcohol.
 - e. The sale of second-hand, or used merchandise, is prohibited.

Chapter 3: East Gateway Code

3.3 Development Standards

3.3 Development Standards

3.3.1 Purpose

This section establishes the development standards to implement the East Gateway Specific Plan for property subject to the East Gateway Code.

3.3.2 General Standards

The following requirements apply to all development within the code boundaries:

1. Buildings, parking, and circulation must be located as allowed by Tables 3A through 3E.
2. Streetscapes and property frontages must comply with the applicable requirements of Table 3F 'Property Frontage Standards'.

3.3.3 Standards and Guidelines by Block

The property within the East Gateway Code boundaries is divided into four distinct areas containing seven individual blocks to address the variety of intended development activity and physical improvements as identified in Figure 2:

Blocks 1 and 2: Table 3A

Blocks 3 and 4: Table 3B

Blocks 5 and 6: Table 3C

Block 7: Table 3D

Tables 3A through 3D identify the development standards and guidelines for each of the seven blocks as follows:

- a. Building Placement and Allowed Encroachments
- b. Parking Placement
- c. Property Frontage and Landscape
- d. Building Height

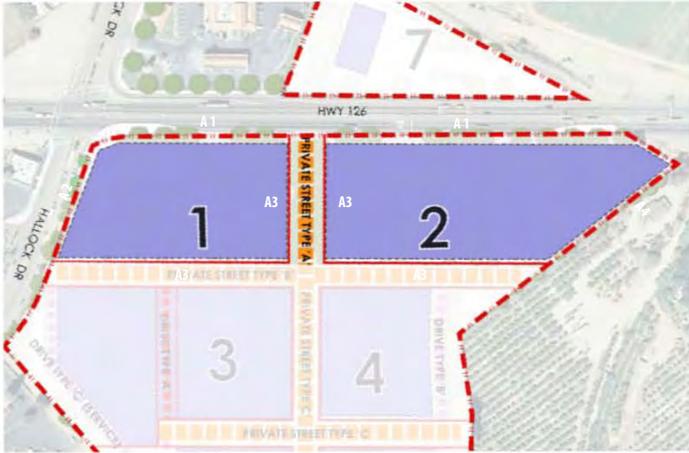
3.3.4 Required Findings

Applications involving Section 3.0 of the East Gateway Code must be consistent with the applicable findings below in order to be approved:

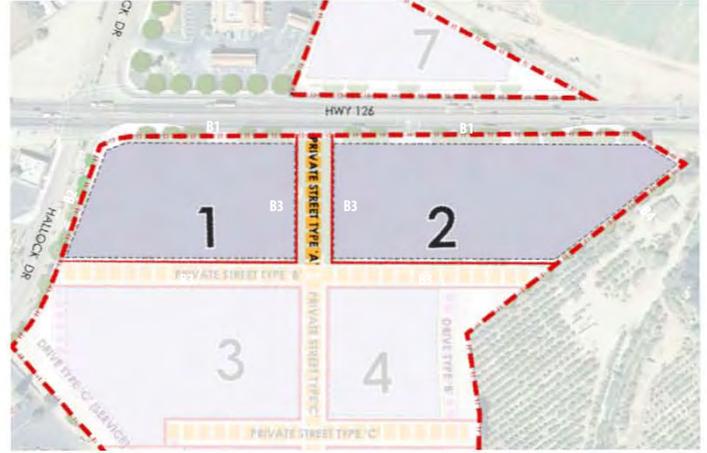
1. "The proposed application..."
 - a. Generates or maintains the streetscape(s) envisioned in the East Gateway Specific Plan through the appropriate design of property frontages;
 - b. Generates or maintains an active and continuous streetscape along all required frontages to support pedestrian activity in combination with vehicular traffic;
 - c. Generates or maintains an appropriate transition from the public streetscape to individual buildings;
 - d. Is in compliance with the requirements of the applicable overlay as identified in Figure 2.
 - e. Expresses creativity in designing frontages per the allowed property frontage types identified in Table 3F.

Chapter 3: East Gateway Code

3.3 Development Standards



Specific Plan boundary
 R.O.W. boundary for Private Interior Street or Drive
 New 'private' street or Drive
 Required Building Setback Line (see Section 'A' below)
 Buildings may only be located within shaded area



Specific Plan boundary
 R.O.W. boundary for Private Interior Street or Drive
 New 'private' street or Drive
 Required Parking Setback Line (see Section 'B' below)
 Parking may only be located within shaded area

A. Required Building Setbacks. Buildings and additions to buildings as well as encroachments must be located within the shaded area in diagram 3A.1 per the following requirements:

B. Required Parking placement. Parking and driveway access must be located in compliance with the following setback requirements and as shown in Diagram 3A.2:

Facade Location	Required Building Setback	Allowed Encroachments into Required Setbacks [a]			
		Gallery	Awning	Eave	Dining Patio
A1 Heritage Valley Hwy [1]	35	5	5	5	none
A2 Hallock Drive	20	5	5	5	none
A3 Private Street A or B	0	within 2 ft of curb		none	
A4 Side or rear to east SP boundary	15	none	5	5	10
A5 Side to nearest Bldg	15	max 10 if 15 ft setback provided			

[a] Notes:
 [1] Gable-roofed building elements (up to 40' in width) may encroach into the required setback by up to 5' provided a majority of the building facade is set back to at least 40'.

2 - Parking and driveways may encroach into setbacks. See 'B' on this page.

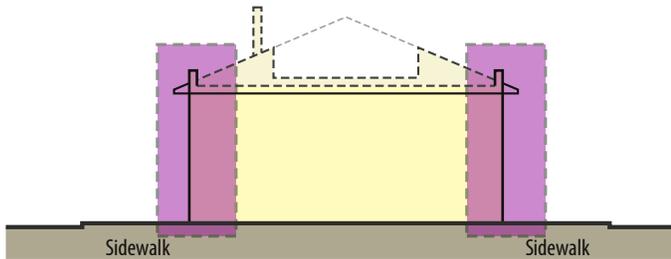
Accessory Structures. Structures and improvements such as screen walls, trash enclosures, bicycle parking and shopping cart collection areas must comply with the requirements of Table 4C.

Area	Required Parking Setback
	Min (ft)
B1 Heritage Valley Hwy	50
B2 Hallock Drive	20
B3 Private Street A or B	5
B4 East Edge	10
B5 Setback from any building	5

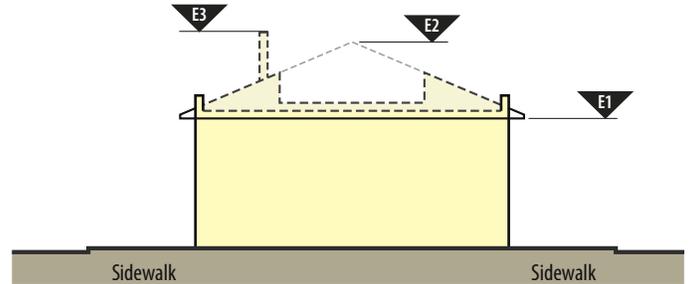
C. Driveway Location and Standards. Driveways and access to individual properties must be provided through the installation of private streets or service drives per the following standards:

1. New streets and individual access to property must be located per Figure 2. Locations may be adjusted by up to 150 in either direction feet subject to City approval and per the minimum dimension range (300-350 ft) as indicated in Figure 2.

2. New streets and individual access to property must be designed per the requirements of Table 3E 'Street Standards'.



 Facade subject to Property Frontage requirements per Section 'D' below.



 Building and Improvements subject to Building Height requirements per Section 'E' below.

Diagram 3A.3:
Property Frontage and Landscaping

D. Property Frontage. Buildings must incorporate one or more of the allowed frontage types into any facade that faces SR 126, a private interior street or, a parking area. The frontage types allowed within blocks 1 and 2 are identified below:

Frontage Type	Allowed Frontage Types by Location		
	Facing Hwy 126 or Hallock Drive	Facing Private Interior Street	Facing Parking Lot
Shopfront (see Table 3F.1)	Allowed	Allowed	Allowed
Gallery (see Table 3F.2)	Allowed	Allowed	Allowed
Forecourt (see Table 3F.3)	Allowed	Allowed	Allowed

Buildings must be designed per the requirements of Table 4A.

Landscaping. Landscaping for blocks 1 and 2 must be provided per the applicable requirements of Figure 3.

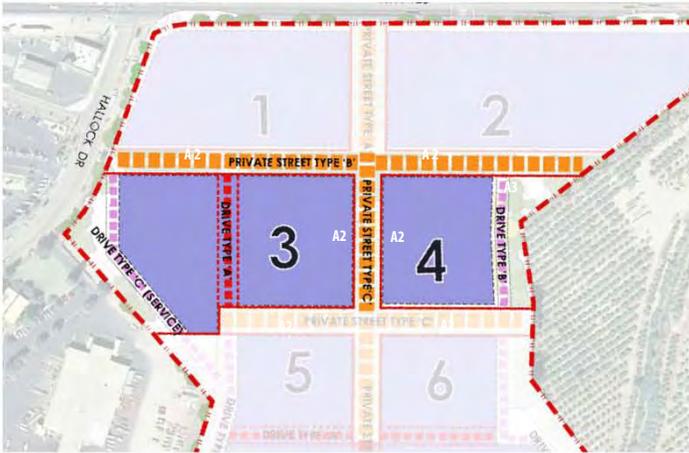
Diagram 3A.4:
Building Height

E. Height limit. Buildings cannot exceed the following limits. Minimum and Maximum height are measured from average finished grade at the adjacent sidewalk.

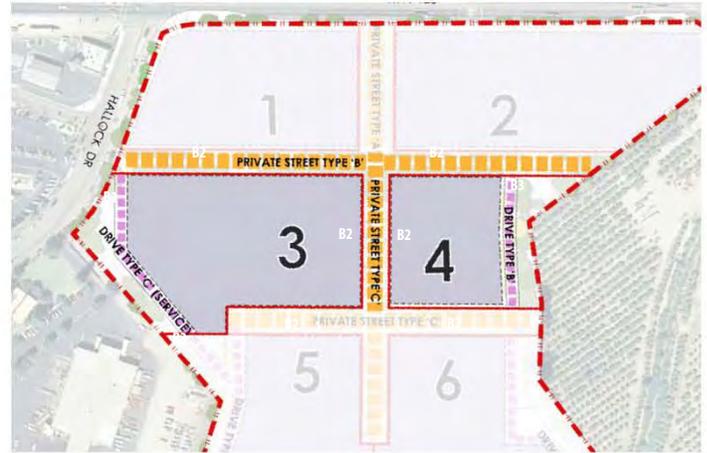
Measurement	Height (in ft)	
	Min	Max
E1 Building Eave	18	25
E2 Roof	no min	35
E3 Architectural Features beyond ridge of roof such as chimneys, dormers, cupolas, weather vanes, roof sign	no min	50

Chapter 3: East Gateway Code

3.3 Development Standards



Specific Plan boundary
 R.O.W. boundary for Private Interior Street or Drive
 New 'private' street or Drive
 Required Building Setback Line (see Section 'A' below)
 Buildings may only be located within shaded area



Specific Plan boundary
 R.O.W. boundary for Private Interior Street or Drive
 New 'private' street or Drive
 Required Parking Setback Line (see Section 'B' below)
 Parking may only be located within shaded area

A. Required Building Setbacks. Buildings and additions to buildings as well as encroachments must be located within the shaded area in diagram 3B.1 per the following requirements:

B. Required Parking placement. Parking and driveway access must be located in compliance with the following setback requirements and as shown in Diagram 3B.2:

Facade Location	Required Building Setback	Allowed Encroachments into Required Setbacks [a]			
		Gallery	Awning	Eave	Dining Patio
A1 Side or Front on Hallock	20	5	5	5	none
A2 Private Street A, B or C	0	within 2 ft of curb			none
A3 Side or rear to west or east SP boundary	15	none	5	5	none
A4 Side to nearest Bldg	15	5	5	5	none

[a] Notes:

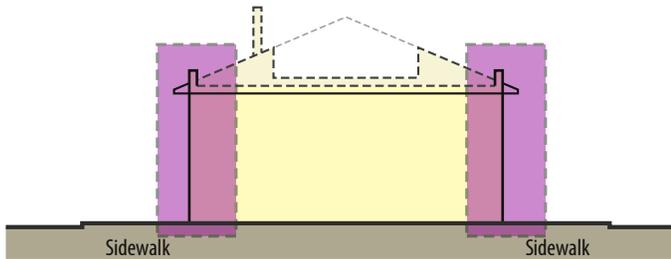
1 - Parking and driveways may encroach into setbacks. See 'B' on this page.

Accessory Structures. Structures and improvements such as screen walls, trash enclosures, bicycle parking and shopping cart collection areas must comply with the requirements of Table 4C.

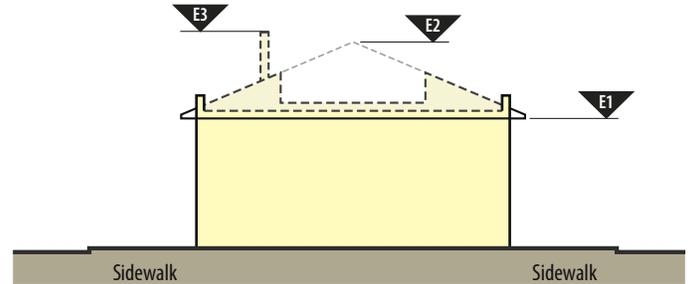
Area	Required Parking Setback
	Min (ft)
B1 Side or Front on Hallock	20
B2 Private Street A, B or C	5
B3 Side or Rear to west or east SP Boundary	10
B4 Setback from any building	5

C. Driveway Location and Standards. Driveways and access to individual properties must be provided through the installation of private streets or service drives per the following standards:

1. New streets and individual access to property must be located per Figure 2. Locations may be adjusted by up to 150 feet subject to City approval and per the minimum dimension range (300-350 ft) as indicated in Figure 2.. Private Street Type 'C' may be adjusted by more in response to the Regional Retail Center Alt 'B'. Private Drive in block 3 not required for Mixed-Use Alt 'A' or 'B'. If blocks 3, 4 are developed before block 1 or 2, the necessary access to Hallock or SR 126 must be installed.
2. New streets and individual access to property must be designed per the requirements of Table 3E 'Street Standards'.



 Facade subject to Property Frontage requirements per Section 'D' below.



 Building and Improvements subject to Building Height requirements per Section 'E' below.

Diagram 3A.3:
Property Frontage and Landscaping

D. Property Frontage. Buildings must incorporate one or more of the allowed frontage types into any facade that faces Hallock Drive, a private interior street or, a parking area. The frontage types allowed within blocks 3 and 4 are identified below:

Frontage Type	Allowed Frontage Types by Location		
	Facing Hallock Drive	Facing Private Interior Street	Facing Parking Lot
Shopfront (see Table 3F.1)	Allowed	Allowed	Allowed
Gallery (see Table 3F.2)	Allowed	Allowed	Allowed
Forecourt (see Table 3F.3)	Allowed	Allowed	Allowed

Buildings must be designed per the requirements of Table 4A.

Landscaping. Landscaping for blocks 3 and 4 must be provided per the applicable requirements of Figure 3.

Diagram 3A.4:
Building Height

E. Height limit. Buildings cannot exceed the following limits. Minimum and Maximum height are measured from average finished grade at the adjacent sidewalk.

Measurement	Height (in ft)	
	Min	Max
E1 Building Eave	18	25
E2 Roof	no min	35
E3 Architectural Features beyond ridge of roof such as chimneys, dormers, cupolas, weather vanes, roof sign	no min	50

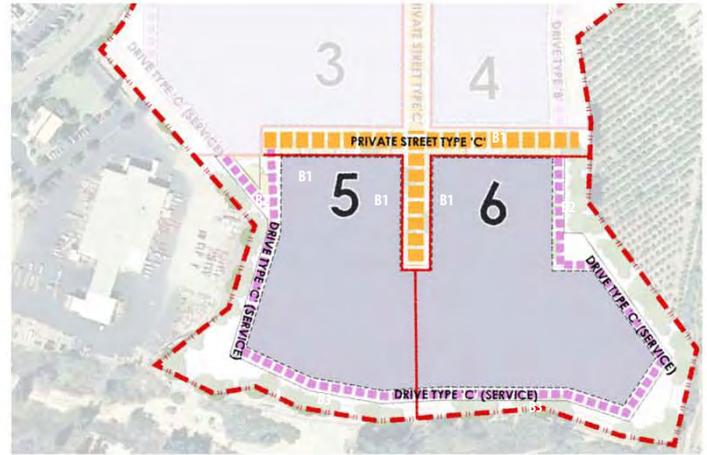
Chapter 3: East Gateway Code

3.3 Development Standards



'B'

- Specific Plan boundary
- R.O.W. boundary for Private Interior Street or Drive
- New 'private' street or Drive
- Required Building Setback Line (see Section 'A' below)
- Buildings may only be located within shaded area



'B'

- Specific Plan boundary
- R.O.W. boundary for Private Interior Street or Drive
- New 'private' street or Drive
- Required Parking Setback Line (see Section 'B' below)
- Parking may only be located within shaded area

A. Required Building Setbacks. Buildings and additions to buildings as well as encroachments must be located within the shaded area in diagram 3C.1 per the following requirements:

B. Parking placement. Parking and driveway access must be located in compliance with the following setback requirements and as shown in Diagram 3C.2:

Facade Location	Required Building Setback	Allowed Encroachments into Required Setbacks [a]			
		Gallery	Awning	Eave	Dining Patio
A1 Private Street A or C	0	within 2 ft of curb			none
A2 Side or rear to west or east SP boundary	15	none	5	5	none
A3 Side or rear to River	20	5	5	5	none
A4 Side to nearest Bldg	0	max 10 when 15 ft side setback			

[a] Notes:

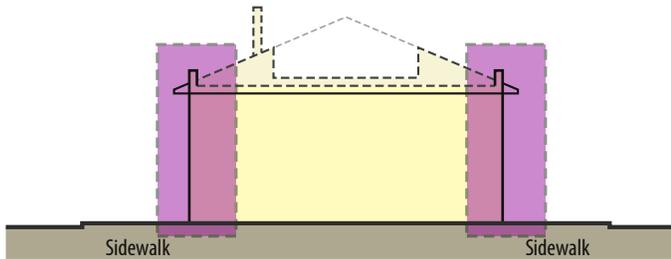
- 1 - Building may be set back further than required when at least 20 feet from the required building setback line.
- 2 - Parking and driveways may encroach into setbacks. See 'B' on this page.

Accessory Structures. Structures and improvements such as screen walls, trash enclosures, bicycle parking and shopping cart collection areas must comply with the requirements of Table 4C.

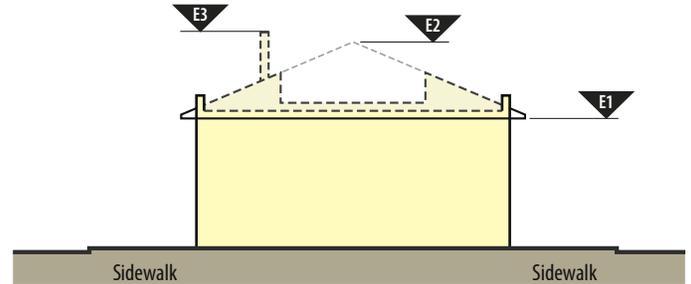
Area	Required Parking Setback (ft)
B1 Private Street A or C	5
B2 Side or Rear to west or east SP boundary	10
B3 Side or Rear to River	10
B4 Setback from any building	5

C. Driveway Location and Standards. Driveways and access to individual properties must be provided through the installation of private streets or service drives per the following standards:

1. New streets and individual access to property must be located per Figure 2. Locations may be adjusted by up to 150 feet subject to City approval and per the minimum dimension range (300-350 ft) as indicated in Figure 2.. Private Street 'A' may be adjusted as necessary in response to Regional Retail Center Alt 'B'.
2. New streets and individual access to property must be designed per the requirements of Table 3E 'Street Standards'.
3. If blocks 5 or 6 are developed before blocks 1 through 4, the necessary access to Hallock Drive or SR 126 must be installed.



 Facade subject to Property Frontage requirements per Section 'D' below.



 Building and Improvements subject to Building Height requirements per Section 'E' below.

Diagram 3A.3:
Property Frontage and Landscaping

D. Property Frontage. Buildings must incorporate one or more of the allowed frontage types into any facade that faces SR 126, a private interior street or, a parking area. The frontage types allowed within blocks 5 and 6 are identified below:

Frontage Type	Allowed Frontage Types by Location		
	Facing Hwy 126	Facing Private Interior Street	Facing Parking Lot
Shopfront (see Table 3F.1)	Allowed	Allowed	Allowed
Gallery (see Table 3F.2)	Allowed	Allowed	Allowed
Forecourt (see Table 3F.3)	Allowed	Allowed	Allowed

Buildings must be designed per the requirements of Table 4A.

Landscaping. Landscaping for blocks 5 and 6 must be provided per the applicable requirements of Figure 3.

Diagram 3A.4:
Building Height

E. Height limit. Buildings cannot exceed the following limits. Minimum and Maximum height are measured from average finished grade at the adjacent sidewalk.

Measurement	Height (in ft)	
	Min	Max
E1 Building Eave	18	40
E2 Roof	no min	55
E3 Architectural Features beyond ridge of roof such as chimneys, dormers, cupolas, weather vanes, roof sign	no min	60

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3.3 Development Standards



Specific Plan boundary
 R.O.W. boundary for Telegraph Road
 West boundary of block
 Required Building Setback Line: (see Section 'A' below)
 Buildings may only be located within shaded area



Specific Plan boundary
 R.O.W. boundary for Telegraph Road
 West boundary of block
 Required Parking Setback Line: (see Section 'b' below)
 Parking may only be located within shaded area

A. Required Setbacks. Buildings and additions to buildings as well as encroachments must be located within the shaded area in diagram 3D.1 per the following requirements:

B. Parking placement. Parking and driveway access must be located in compliance with the following setback requirements and as shown in Diagram 3D.2:

Facade Location	Required Building Setback Min (ft)	Allowed Encroachments into Required Setbacks [a]			
		Gallery	Awning	Eave	Dining Patio
A1 Heritage Valley Hwy	30	5	5	5	10
A2 East City entry	200	20	10	10	100
A3 Telegraph Road	20	5	5	5	10
A4 Side or Rear to nearest Bldg	15	none	5	5	none

[a] Notes:

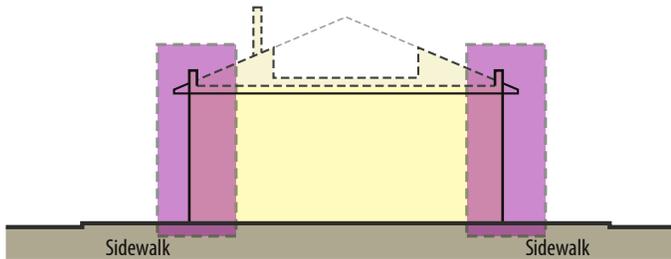
1 - Parking and driveways may encroach into setbacks. See 'B' on this page.

Accessory Structures. Structures and improvements such as screen walls, trash enclosures, bicycle parking and shopping cart collection areas must comply with the requirements of Table 4C.

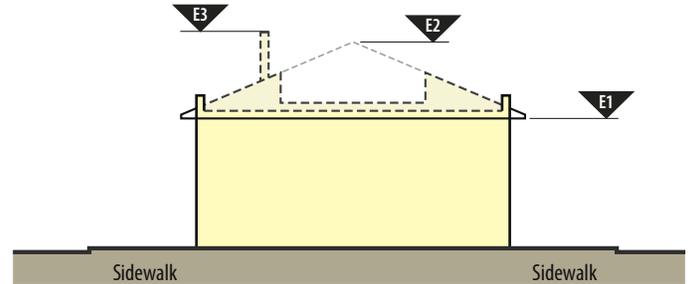
Area	Required Parking Setback Min (ft)
B1 Heritage Valley Hwy	30
B2 East City entry	30
B3 Telegraph Road	15
B4 Setback from any building	5

C. Driveway Location and Standards. Driveways and access to individual properties must be provided through the installation of private streets or service per the following standards:

- Individual access to property must be located per Figure 2. Locations may be adjusted by up to 150 feet subject to City approval.
- New streets and individual access to property must be designed per the requirements of Table 3E 'Street Standards'.



 Facade subject to Property Frontage requirements per Section 'D' below.



 Building and Improvements subject to Building Height requirements per Section 'E' below.

Diagram 3D.3:
Property Frontage and Landscaping

D. Property Frontage. Buildings must incorporate one or more of the allowed frontage types into any facade that faces SR 126, Telegraph Road or, a parking area. The frontage types allowed within block 7 are identified below:

Frontage Type	Allowed Frontage Types by Location		
	Facing Hwy 126	Facing Private Telegraph Rd	Facing Parking Lot
Shopfront (see Table 3F.1)	Allowed	Allowed	Allowed
Gallery (see Table 3F.2)	Allowed	Allowed	Allowed
Forecourt (see Table 3F.3)	Allowed	Allowed	Allowed

Buildings must be designed per the requirements of Table 4A.

Landscaping. Landscaping for block 7 must be provided per the applicable requirements of Figure 3.

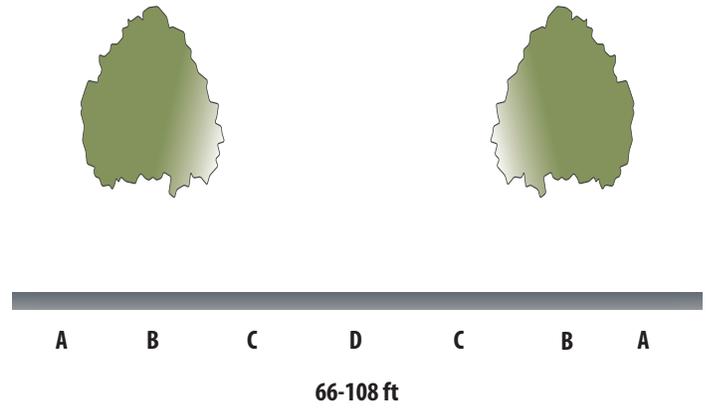
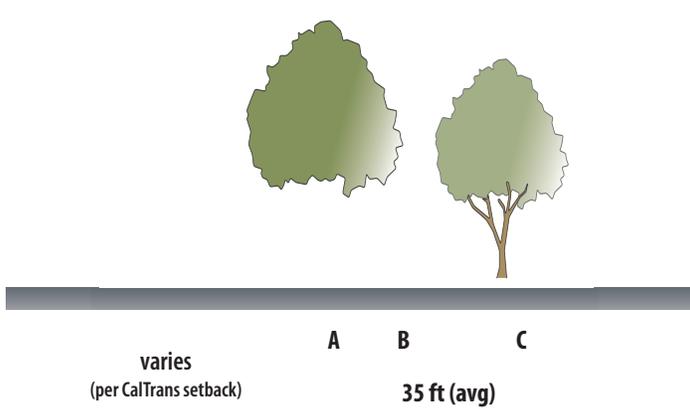
Diagram 3D.4:
Building Height

E. Height limit. Buildings cannot exceed the following limits. Minimum and Maximum height are measured from average finished grade at the adjacent sidewalk.

Measurement	Height (in ft)	
	Min	Max
E1 Building Eave	18	25
E2 Roof	no min	35
E3 Architectural Features beyond ridge of roof such as chimneys, dormers, cupolas, weather vanes, roof sign	no min	40

Chapter 3: East Gateway Code

3.3 Development Standards



Configuration. Improvements within the above right-of-way must be in compliance with the applicable requirements identified below:

Alignment. The actual alignment and location of Private Street Type 'A' may be adjusted from the location shown in Figure 2 by up to the amount shown in Tables 3A-3D; Section 'C' - Driveway Location and Standards' for the applicable block(s).

Configuration. Improvements within the above right-of-way must be in compliance with the applicable requirements identified below:

Component of Street	Requirement	
	Min ft	Max ft
A Parkway / Planter	6'	none
B Sidewalk / Bikeway	8'	12'
C Parkway / Planter / Lawn	15' [1]	none

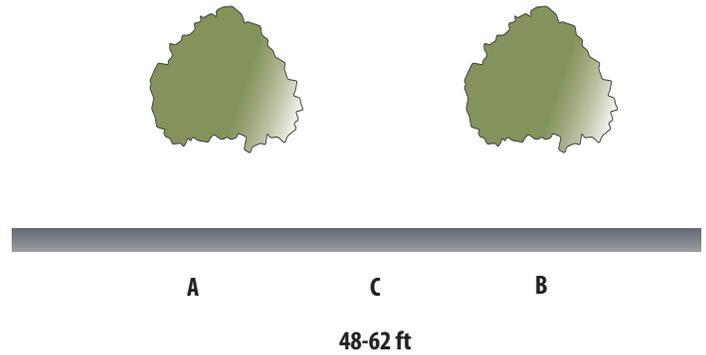
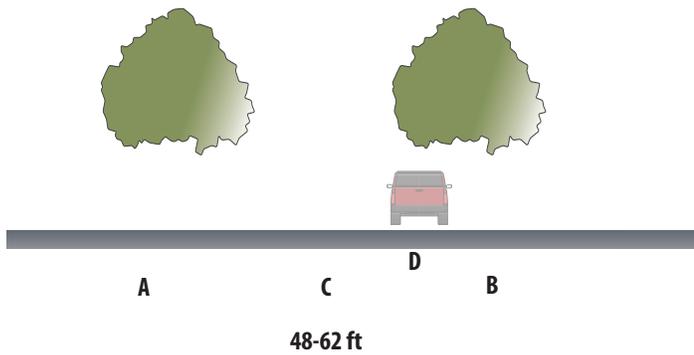
Landscaping. Landscaping for the above right-of-way must be provided per the applicable requirements of Figure 3.

[1] In order to achieve massing articulation, 1-story gable-roofed building elements (up to 40' in width) may encroach into the required setback by up to 5' provided a majority of the building facade is set back to at least 40'.

Component of Street	Requirement	
	Min ft	Max ft
A Sidewalk [1]	6'	12'
B Parkway / Hedgegrove	10'	14'
C Drive Lane	12'	18'
S Planted Median / Bioswale	10'	20'

Landscaping. Landscaping for the above right-of-way must be provided per the applicable requirements of Figure 3.

[1] A landscape strip of 4'-6' may be provided between sidewalk and building. In such case a minimum 6' sidewalk must be provided.



Alignment. The actual alignment and location of Private Street ‘B’ may be adjusted from the location shown in Figure 2 by up to the amount shown in Tables 3A-3D; Section ‘C’ - Driveway Location and Standards’ for the applicable block(s).

Configuration. Improvements within the above right-of-way must be in compliance with the applicable requirements identified below:

Component of Street	Requirement	
	Min ft	Max ft
A Sidewalk and Planter (where occurs)	12’	16’
B Drive Lanes	24’	30’ [1]
C Sidewalk and Planter (where occurs)	12’	16’
D Optional Curb-side parking / passenger drop-off area.	7’	10’

Landscaping. Landscaping for the above right-of-way must be provided per the applicable requirements of Figure 3.

[1] 30’ required if optional curbside parking is provided.

Alignment. The actual alignment and location of Private Street ‘C’ may be adjusted from the location shown in Figure 2 by up to the amount shown in Tables 3A-3D; Section ‘C’ - Driveway Location and Standards’ for the applicable block(s).

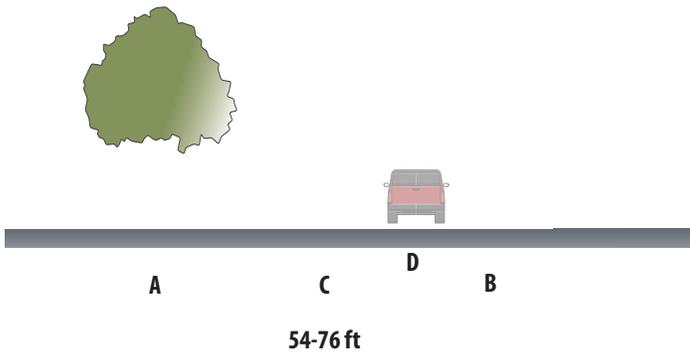
Configuration. Improvements within the above right-of-way must be in compliance with the applicable requirements identified below:

Component of Street	Requirement	
	Min ft	Max ft
A Sidewalk and Planter (where occurs)	12’	16’
B Drive Lanes	24’	30’
C Parkway (Parking Lot Buffer)	12’	16’

Landscaping. Landscaping for the above right-of-way must be provided per the applicable requirements of Figure 3.

Chapter 3: East Gateway Code

3.3 Development Standards



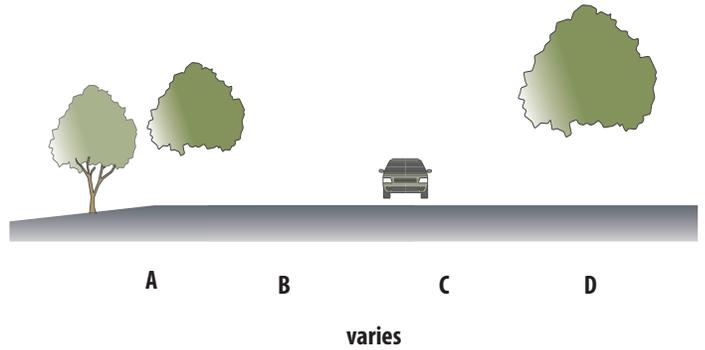
Alignment. The actual alignment and location of Drive Type 'A' may be adjusted from the location shown in Figure 2 by up to the amount shown in Tables 3A-3D; Section 'C' - Driveway Location and Standards' for the applicable block(s).

Configuration. Improvements within the above right-of-way must be in compliance with the applicable requirements identified below:

Component of Street	Requirement	
	Min ft	Max ft
A Parkway	8'	none
B Sidewalk and Shopfront	14'	20'
C Drive Lanes	24'	30' [1]
D Optional curb-side parking and passenger drop-off area	8'	10'

[1] 30' required if optional curbside parking is provided.

Landscaping. Landscaping for the above right-of-way must be provided per the applicable requirements of Figure 3.

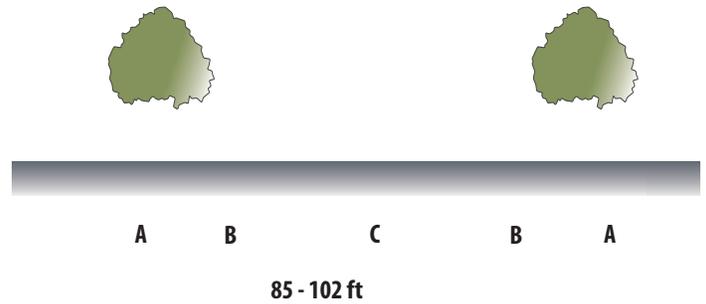
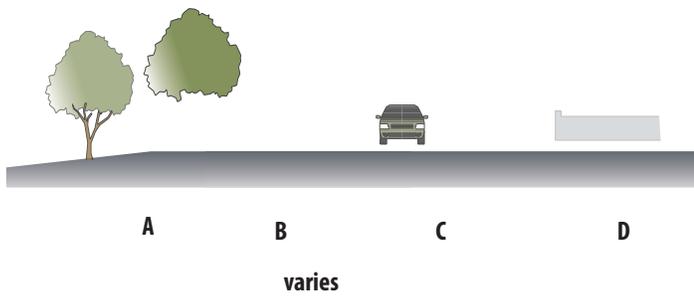


Alignment. The actual alignment and location of Drive Type 'B' may be adjusted from the location shown in Figure 2 by up to the amount shown in Tables 3A-3D; Section 'C' - Driveway Location and Standards' for the applicable block(s).

Configuration. Improvements within the above right-of-way must be in compliance with the applicable requirements identified below:

Component of Street	Requirement	
	Min ft	Max ft
A South Edge	20'	none
B Parking Area (optional)	18'	20'
C Drive Lanes	20'	30'
D Parkway	8'	none

Landscaping. Landscaping for the above right-of-way must be provided per the applicable requirements of Figure 3.



Alignment. The actual alignment and location of Drive Type ‘C’ (Service) may be adjusted from the location shown in Figure 2 by up to the amount shown in Tables 3A-3D; Section ‘C’ - Driveway Location and Standards’ for the applicable block(s).

Configuration. Improvements within the above right-of-way must be in compliance with the applicable requirements identified below:

Component of Street	Requirement	
	Min ft	Max ft
A Perimeter Edge (Curb to Natural Area)	20’	none
B Parking Area (Optional)	18’	20’
C Drive Lanes	20’	30’
D Loading Area or Additional Prking	varies	none

Landscaping. Landscaping for the above right-of-way must be provided per the applicable requirements of Figure 3.

Alignment. The actual alignment and location of Drive Type ‘C’ (Service) may be adjusted from the location shown in Figure 2 by up to the amount shown in Tables 3A-3D; Section ‘C’ - Driveway Location and Standards’ for the applicable block(s).

Configuration. Improvements within the above right-of-way must be in compliance with the applicable requirements identified below:

Component of Street	Requirement	
	Min ft	Max ft
A Sidewalk and Planter	12’	16’
B Head-in / Diagonal Parking	18’	20’
C Drive Lanes [2]	25’	30’

Landscaping. Landscaping for the above right-of-way must be provided per the applicable requirements of Figure 3.

[1] This section applies to Mixed-Use District Alternatives ‘C’ & ‘D’ only.

[2] Standard dimension to regulate drive lanes for all parking areas.

Chapter 3: East Gateway Code

3.3 Development Standards

Description and Intent. Shopfront and Awning frontages are created by inserting storefronts with substantial glazing into the ground floor facade of a building. The facade is aligned with the property line, although partially recessed storefronts, such as recessed entrances, are also common. The building entrance is at sidewalk grade and provides direct access to a non-residential ground floor use.

Standards.

1. Shopfronts are composed of storefronts, entrances, awnings or sheds, signage, lighting, cornices, and other architectural elements. See Shopfront assembly diagram on this page.
2. Shopfronts must be at least 10 feet tall as measured from the adjacent sidewalk to the top of the storefront opening.
3. Where used, a shopfront must comprise at least 50% of the facade except on blocks 5 and 6 where the minimum requirement is 15% of the facade.
4. A solid base or bulkhead must be provided with a maximum height of 24 inches above sidewalk grade.
5. For the area of the ground floor facade that is between 2 and 10 feet, at least 70% of this area must consist of transparent fenestration.
6. Awnings or sheds may encroach into the public right-of-way and cover the sidewalk to within 2 feet of the curb. Awnings, sheds, signage or other sidewalk encroachments must be at minimum 8 feet above sidewalk grade.

Public R.O.W.

Private Lot

10' min



2' max bulkhead

Typical cross section of Shopfront and Awning frontage.



Axonometric view of Shopfront and Awning frontage



Example of a Shopfront & Awning frontage



Example of a Shopfront & Awning frontage

Description and Intent. Galleries are created by attaching a colonnade to a building facade that is aligned with or near the property line and typically contains ground-floor storefronts. The colonnade projects over the sidewalk and encroaches into the public right-of-way. This frontage type is ideal for retail use.

Standards.

1. Galleries must provide at least 8 feet clear between the facade and the inside of the posts or columns.
2. The space between the face of the curb and the outside face of the posts or columns must be between 24 and 30 inches to provide sufficient room for overhanging bumpers and to encourage walking along the inside of the Gallery.

Public R.O.W.

Private Lot



Typical cross section of Gallery frontage.



Street view of Gallery frontage



Example of a Gallery frontage



Example of a Gallery frontage

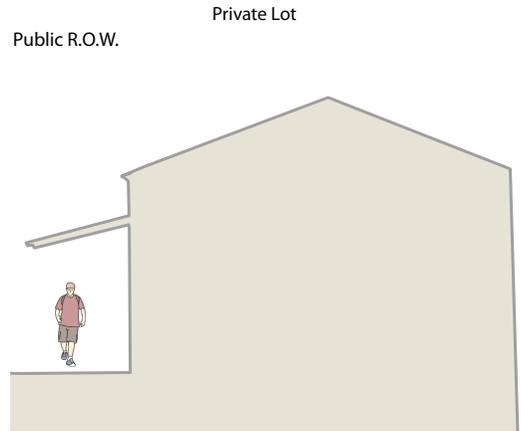
Chapter 3: East Gateway Code

3.3 Development Standards

Description and Intent. Forecourts are created by setting back all or a portion of a building's facade, typically the central portion. Forecourts typically provide access to a central lobby of a larger building, but may also be combined with other frontage types that provide direct access to the portions of the facade that are close to the sidewalk. Larger Forecourts may allow for vehicular access.

Standards.

1. Forecourts may be landscaped or paved.
2. Forecourts may be at grade or elevated above the sidewalk a maximum of 24 inches.
3. Forecourts must be at least 10 feet in width and depth. The width of a Forecourt cannot exceed 1/3 of the overall facade width, and the depth must be equal to or less than the width.



Typical cross section of Forecourt frontage.



Axonometric view of Forecourt frontage



Example of a Forecourt frontage



Example of a Forecourt frontage

3.4 Building Design and Landscape Standards

3.4.1 Purpose

This section establishes standards for facade and building massing, landscaping, lighting and the location of utility services for property and rights-of-way subject to the East Gateway Code.

3.4.2 General Standards and Guidelines

1. Property and rights-of-way subject to the East Gateway Code must comply with the applicable requirements of Tables 4A, 4B and 4C.
2. Tables 4A through 4C identify the development standards and guidelines as follows:
 - a. Building Massing and Facades (Table 4A)
 - b. Landscape and Plant Materials (Table 4B)
 - c. Lighting, Services, Utilities (Table 4C)

3.4.3 Required Findings

Applications involving Section 3.4 of the East Gateway Code must be consistent with the applicable findings below in order to be approved:

1. "The proposed application..."
 - a. Generates or maintains an interconnected network of appealing streetscapes through building designs which are consistent with the intended physical character;
 - b. Facilitates appealing building design with building entrances on bordering streets and/or open space;
 - c. Provides for vehicular/service access to maintain appealing streetscapes while adequately servicing the individual buildings;
 - d. Complies with the applicable requirements of Figure 2.
 - e. Is in compliance with the applicable building massing and facade requirements.

Chapter 3: East Gateway Code

3.4 Building Design and Landscape Standards

TABLE 4A: BUILDING MASSING AND FACADES

3.4.4 Standards and Guidelines

Buildings and modifications or additions to buildings subject to the East Gateway Code must comply with the following standards and guidelines, as applicable. Guidelines are distinct from standards through the use of 'should' versus 'must':

1. Each building should be sited to have the eave of its primary facade be generally parallel or parallel to the nearest adjacent frontage.
2. Ground floor facades must comply with the applicable property frontage requirements.
3. Each building should reflect the architectural character of buildings at the City's edge per the East Gateway Specific Plan.
4. Buildings must be in compliance with the following requirements:
 - Section 3.4.4A Building Entrances
 - Section 3.4.4B Building Materials and Finishes

3.4.4A Building Entrances

- a. The primary entrance should be distinguished by architectural features such as, without limitation: an entry portal; change in material or color; change in scale of other openings; addition of columns, lintels or canopies. Such features must be consistent with the architectural style of the building to which they are attached.
- b. Secondary entrances should have architectural features that are smaller in height and width, and have fewer or simpler architectural elements than the primary entrance in scale and detail.
- c. Entry doors cannot swing out to the sidewalk unless the door when fully open does not encroach into the sidewalk.
- d. Entries to buildings must be at the same grade as the sidewalk nearest the building.
- e. Handrails or other such improvements cannot block visibility of the building entry or direct access from the sidewalk.

3.4.4B Building Materials and Finishes

- a. Windows, doors, frames, colors, and styles must be appropriate to the building's architectural style;
- b. Allowed window and door materials include wood, fiberglass, steel, or aluminum. Vinyl and vinyl-clad windows are allowed with mullion patterns and colors appropriate to the building's architectural style;
- c. Glazing must be clear glass, particularly in storefront and primary window applications. Transom and other specialty windows may be decorative.
- d. Ground floor facades must be distinct from upper floors through the use or finish of materials, colors, window sizes, and architectural details.
- e. Upper floor cladding materials must generally be organized with heavier appearing materials lower on the wall in relation to lighter appearing materials.

- f. Window systems or other such systems where more than 50% of the wall surface on upper floors and more than 80% of the ground floor wall surface consists of glass are not allowed.
- g. Materials must be durable and detailed to complement the streetscape.
- h. Exposed wood (or wood-like materials) should be finished in a manner that minimizes maintenance and promotes the material's longevity.
- i. The use of reflective materials is allowed when:
 - i. The material is applied to small areas such as to highlight signage; and;
 - ii. The material will not cause a nuisance to automobile traffic, pedestrians, and neighboring buildings.
 - iii. Active solar devices must be fully integrated into the overall form of the building. Passive solar devices such as overhangs, shutters, louvers, canopies, and shade trees must be used to minimize solar heat gain.
- j. Masonry veneer walls must be detailed with structural integrity, appearing thicker and heavier, especially at corners and window and door openings.
- k. Brick and cut stone must be laid in true bonding patterns. Mortar joints must be struck.
- l. River and rubble stone must be laid from large to small, with smooth or beaded mortar joints.
- m. Masonry detailing must involve the transition from stone to other materials through moldings, caps, and other trim elements.
- n. The use of synthetic materials is allowed when the material:
 - i. Adequately simulates the appearance of the natural material it imitates;
 - ii. Demonstrates an ability to age similar to or better than the natural material it imitates;
 - iii. Has a permanent texture, color, and character that is acceptable for the proposed application; and
 - iv. Can be pressure washed and, in general, withstand anti-graffiti measures.
- o. Two or more wall materials may be combined on one facade. If located one above the other, lighter materials must be placed above more substantial materials (e.g. wood above stucco or masonry, or stucco and glass above masonry). In general, vertical joints between different materials can occur only at inside corners.
- p. Materials and finishes should be composed to provide visually balanced compositions.
- q. Finishes, fixtures, and other architectural details should be designed to be consistent with each other throughout the building.
- r. Attached architectural elements and details such as lighting fixtures, attic vents, custom signage, awnings, hand rails, balconies, and trellises be designed to be consistent with other elements throughout the building.
- s. Accessories must comply with the following:
 - i. Security gates cannot be located outside of a storefront or building facade.
 - ii. Alarm boxes and other such equipment should be placed at least 12 feet above the sidewalk.
- t. Alterations, restoration, rehabilitation, ordinary repair and maintenance must comply with the applicable requirements of the East Gateway Code.

Chapter 3: East Gateway Code

3.4 Building Design and Landscape Standards

3.4.5 Standards and Guidelines

Site landscaping and modifications or additions to site landscaping subject to the East Gateway Code must comply with the following standards and guidelines, as applicable, per Figure 3. Guidelines are distinct from standards through the use of 'should' versus 'must':

1. Each building site and parking area must be landscaped per the applicable requirements of Table 4B.
2. Landscape materials should reflect the architectural character of development at the City's edge per the East Gateway Specific Plan.
3. Landscape areas must be in compliance with the following:
 - Section 3.4.6A Site Landscaping
 - Section 3.4.6B Streetscape Landscaping
 - Section 3.4.6C Parking Area Landscaping

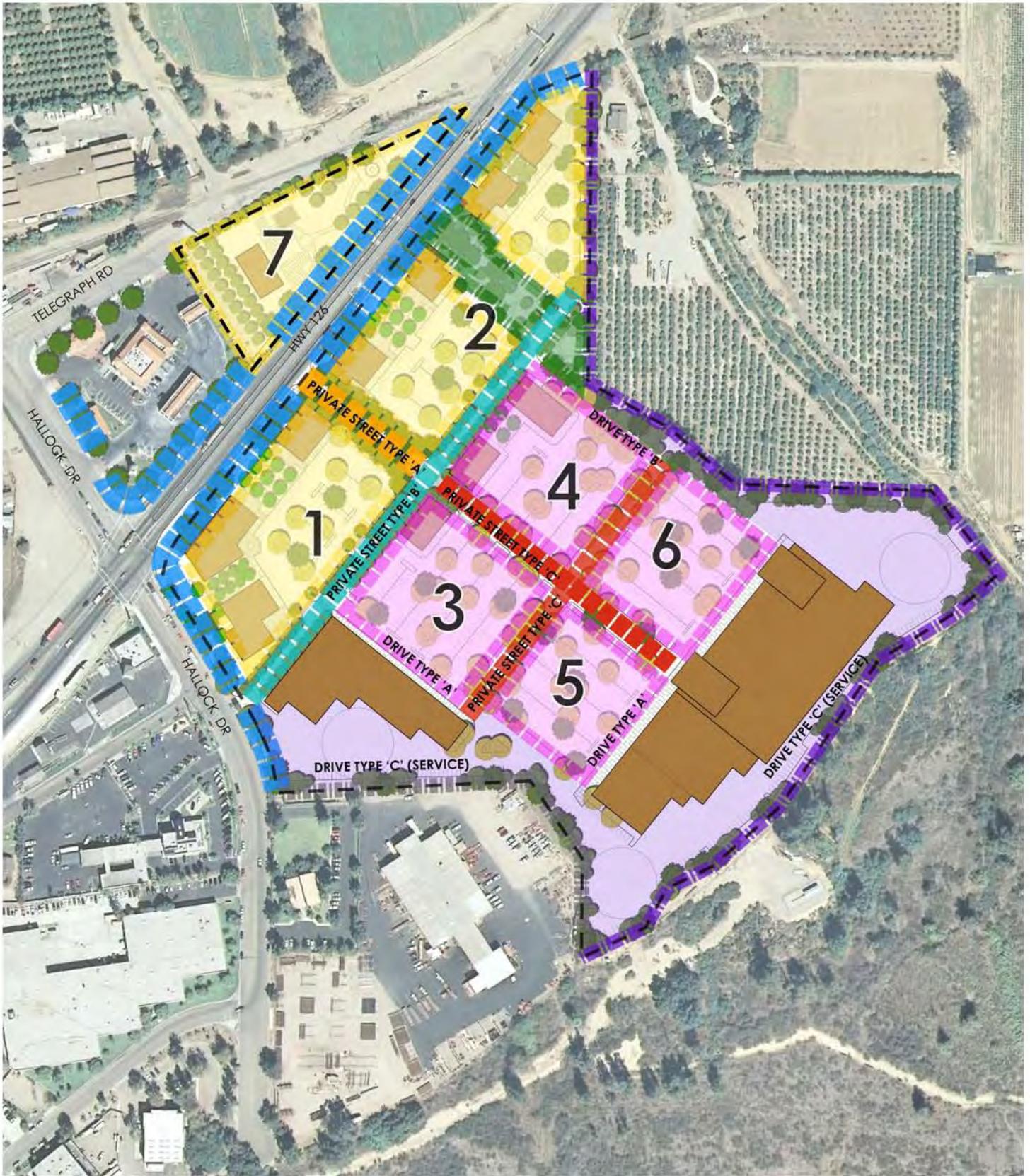
3.4.6 Intended Landscape Character

Landscaping along streetscapes and surrounding individual buildings must comply with the intended landscape character for each area identified in Figure 3 and as described in Table 4B.

Key to Figure 3

-  Heritage Valley Highway Frontage (See 3-39)
-  Hallock Frontage (See 3-40)
-  Private Street Type 'A' (See 3-41)
-  Private Street Types 'B' & 'C' (See 3-42)
-  Greenbelt Edge (See 3-44)
-  Industrial Edge (See 3-44)
-  Courtyard and Parking Areas (as drawn) (See 3-43)

Figure 3-2: Regulating Plan: Zoning



Chapter 3: East Gateway Code

3.4 Building Design and Landscape Standards



Intended character along HWY 126 frontage.

Intended Character: The Heritage Valley Highway is an important State Highway connecting the coastal and inland communities, and serves as the primary - essentially the only - eastern entry to the City of Santa Paula. Accordingly, the landscape character of the highway as it leaves the natural and agricultural environment of the Santa Paula/Fillmore Greenbelt and enters the City of Santa Paula defines the character and identity of the City for all visitors. Per the City's General Plan that character will reflect a transition from the rural to a town environment.

A low alluvial native stone filled gabion wall or split rail fence separates the state highway from a bike trail and unifies the low landscape frontage of the East Gateway landscape. This trail is the most easterly segment of the regional trail planned along the railroad right-of-way from San Buenaventura to Santa Paula and onward to the east.

The landscape character is reflective of the rural/ranch typology of the region. Informal groupings of trees are strategically placed to emphasize the building frontages and signage. Informal groupings of large open canopied native and naturalized trees soften the highway, yet allow visibility into retail buildings. The ground plane ranges from low to tall drought tolerant landscape including vines that reinforces or softens the building massing.

Building arrangements allow for courtyards and dining patios that face the highway. These areas offer the opportunity to reflect the rich ranch industry of the area through formal groupings of trees that can be seen from the highway.



Intended landscape character along Hwy 126



Platanus Racemosa



Schinus Molle

Primary Street Tree:

(Scientific Name / Common Name)

Platanus racemosa / California Sycamore

Secondary Street Tree:

(Scientific Name / Common Name)

Schinus molle / California Pepper

Shrubs and Groundcovers:

(Scientific Name / Common Name)

Calycanthus occidentalis / Carolina sweetshrub

Carpenteria californica / Bush-anemone

Cistus salvifolius / Sage-leaved Rock Rose

Festuca rubra / Red fescue

Nerium oleander 'Petite' / Dwarf Pink Oleander

Rhamnus californica / California buckthorn



Fraxinus velutina



Arbutus 'Marina'



Bougainvillea 'Rosenka'

Intended Character: Hallock Drive is the north-south connector/avenue linking the southerly area of East Area 2 across Highway to the northly portion of East Area 2, East Area 1, and the rest of the City. The landscape character of Hallock along the westerly frontage of the East Gateway Plan Area is to be substantially the same as the Heritage Valley Highway frontage, providing a unified image for this key commercial corner.



Lavandula stoechas



Rosmarinus Officinalis

Primary Street Tree:

(Scientific Name / Common Name)

Fraxinus velutina / Ash

Secondary Street Tree:

(Scientific Name / Common Name)

Arbutus 'Marina' / Marina Strawberry Madrone

Shrubs and Groundcovers:

(Scientific Name / Common Name)

Bougainvillea 'Rosenka' / Rosenka Bougainvillea

Echium candicans / Pride of Madeira

Gazania hybrids / Gazania

Lavandula stoechas / Spanish lavender

Leonotis leonurus / Lion's Tail

Rosmarinus officinalis / Rosemary

Salvia leucantha / Mexican bush sage

Chapter 3: East Gateway Code

3.1 Administration



Intended character of Private Street Type 'A'

Intended Character: The main entry street from SR 126 (Private Street Type 'A') most dramatically emphasizes the rural/ranch imagery. Large canopied native trees flank a eucalyptus-planted median, similar in character to the existing eucalyptus windrows separating numerous agricultural fields along SR 126. The median serves as an entry skyline marquee from great distances. Flanked with a similar windrow, envisioned for the western end of Block 7 to the north of SR 126, both work in concert as an informal entry into Santa Paula. The entry roadway is curbless and storm water is directed into the wide landscape areas. The ground plane is intended to be low, drought tolerant ground cover.



Intended character of Private Street Type 'A' (Heritage Valley Highway entry)



Platanus Racemosa



Eucalyptus camaldulensis

Primary Parkway Tree:

(Scientific Name / Common Name)

Platanus racemosa / California Sycamore

Alternatives:

Acacia stenophylla / Shoestring acacia

Eucalyptus cladocalyx / Sugar Gum

Eucalyptus robusta / swamp mahogany

Geijera parviflora / Native Willow

Platanus racemosa / California sycamore

Primary Median Tree:

(Scientific Name / Common Name)

Eucalyptus camaldulensis / Red River Gum

Alternatives:

Citrus sinensis 'Valencia' / Valencia Orange

Alternates:

Platanus racemosa / California sycamore

Shrubs and Groundcovers:

(Scientific Name / Common Name)

Alyogyne huegelii / Lilac Hibiscus

Anigozanthos / Catpaw

Callistemon viminalis / Weeping Bottlebrush

Grevillea 'Noellii' / Spider flower

Hakea suaveolens / Sweet Hakea

Leptospermum laevigatum / Coastal Tea Tree

Melaleuca armillaris / Bracelet honey myrtle

Senna artemisioides / Feathery Cassia

Senna phyllodinea / Silver Leaf Cassia

Westringia fruticosa / Coast Rosemary



Geijera parvifolia



Acacia farnesiana



Agave desmettiana

Intended Character: The second major entry connects to Hallock Drive. The tree spacing and layout follow a traditional landscape geometry of rows of rustic trees to enclose the driveway. Trees will be planted in a parkway strip between the curb and a sidewalk on at least one side, with planters flanking a parking area on the opposite side. Ground plantings in the parkways and planters will be low to medium native and drought tolerant plant materials that reinforce the rural edge design character.

Additional streets and drives within the Plan area are intended to serve the conventional function of primary drive aisles within and interconnected network of parking areas for this commercial center, while also reinforcing the simple edge-of-town, semi-rural design character of the East Gateway.

This intention will be implemented with simple and subtle design strategies, including the informal spacing of classic rural California tree species, low and moderate ground plantings of native and drought tolerant species, and curbless - or curb inlet - edge designs that allow stormwater to flow into or through planting areas. Where possible, planting area edges may be defined by rows of river cobbles or other natural materials in lieu of standard urban concrete curbs.



Lavandula stoechas



Hesperaloe parviflora

Primary Street Tree:

(Scientific Name / Common Name)

Geijera parviflora / Australian Willow

Alternatives:

Acacia farnesiana / Needle Bush

Chitalpa tashkentensis / Morning Cloud

Parkinsonia aculeata / Mexican palo verde

Prosopis glandulosa / Honey mesquite

Shrubs and Groundcovers:

(Scientific Name / Common Name)

Agave desmettiana / Dwarf Century Plant

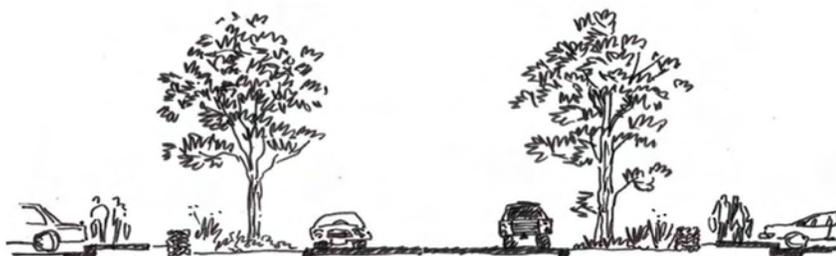
Caesalpinia gilliesii / Bird of paradise

Ceanothus gloriosus / Point Reyes ceanothu

Hesperaloe parviflora / Red Yucca

Muhlenbergia capillaris / Hairawn muhly

Verbena rigida / Rough Verbena



Intended character of Private Street Types 'B' (Hallock St entry) and 'C' (interior circulation streets)

Chapter 3: East Gateway Code

3.4 Building Design and Landscape Standards



Intended landscape character for all parking areas

Intended Character (Parking Lots): The parking lot landscaping is a departure from the small planter “grove like” landscapes found in typical lots (See 3.4.6C). The approach is to emphasize large landscape areas that can serve as bioswales and provide root volumes to support very large trees. Random placement of large specimen and quick growing trees suggests a parking lot designed around the trees. The sustainable principle of large uncompacted root volumes encourages large and tall canopies casting shade over the larger areas. [Refer to 3.4.6 C for specifications]

Trees can be strategically located to provide maximum shade for automobiles while maintaining sightlines into shopfronts.

Intended Character (Forecourts & Courtyards): The courtyards consist of gridded plantings of citrus to reflect the farming culture of the community. Seating areas and dining areas can be incorporated into the citrus groves. Landscapes surrounding buildings include plantings of flowering trees, shrubs vines on walls, pergolas and ground covers to enhance the pedestrian experience.



Intended character of dining courtyard areas

Primary Parking Area Trees:

(Scientific Name / Common Name)

- Juglans californica / California Black Walnut
- Platanus racemosa / California Sycamore
- Quercus agrifolia / Coast Live Oak
- Quercus virginiana / Southern Live Oak
- Quercus engelmannii / Mesa Oak
- Schinus molle / California Pepper

Primary Courtyard Area Trees:

(Scientific Name / Common Name)

- Citrus sinensis ‘Valencia’ / Valencia Orange tree
- Citrus sinensis ‘Washington’ / Washington Orange Tree

Primary Building Adjacent Trees:

(Scientific Name / Common Name)

- Chitalpa tashkentensis / Pink Dawn
- Arbutus ‘Marina’ / Marina Strawberry Tree

Shrubs and Groundcovers:

Courtyards	Parking Areas
Achillea millefolium	Agave americana
Adiantum capillus-veneris	Alyogyne huegelii
Baccharis pilularis	Callistemon viminalis
Berberis aquifolium	Calycanthus occidentalis
Bougainvillea ‘Rosenka’	Carpenteria californica
Carpenteria californica	Ceanothus spp.
Dendromecon rigida	Dendromecon rigida
Dudleya brittonii	Echium candicans
Dudleya lanceolata	Gazania hybrids
Fragaria californica	Grevillea ‘Noellii’
Heuchera hybrids	Hakea suaveolens
Nassella pulchra	Leonotis leonurus
Salvia greggii	Muhlenbergia capillaris
Sisyrinchium bellum	Nassella pulchra
Woodwardia fimbriata	Rosmarinus officinalis
	Salvia spp.
	Westringia fruticosa



Intended landscape character of greenbelt and agricultural edges

Intended Character (Greenbelt Edge): The greenbelt is a riparian landscape of native and naturalized non-native plants from the region. Decomposed paths follow the edge of the greenbelt with seating and picnic areas provided for bike/hiking trail enthusiasts to relax and visit the local retail and food establishments.

Intended Character (Agricultural / River Edge): The perimeter landscape abutting agricultural land and the Santa Clara River flood plain is intended to blend with the existing natural landscape and hold the slope as needed based on flood area recommendations pertaining to grading parts of the site. Plant material is per the Ventura County Fire Authority recommendations for plant types and maintenance.

Intended Character (Industrial Edge): The final edge type is the boundary between the East Gateway Plan Area and the abutting industrial property to the south and west. The primary purpose of landscape along this edge is to screen those industrial operations from views within the East Gateway area. Depending on the used types and building configuration along this edge of the East Gateway site - which could range from loading areas for large retail buildings to parking areas for smaller R&D facilities - the aesthetic requirements for such screening may vary.

In all cases a solid masonry wall, between 6 and 8 feet in height is recommended along this boundary to within approximately 30 feet of Hallock Drive. Ground plantings in front of the wall should be similar to those in other parking areas, with vines covering the wall and parking lot trees planted along the edge to provide a consistent row to screen additional views.



Platanus Racemosa



Alnus rhombifolia

Primary Trees:

(Scientific Name / Common Name)

Platanus racemosa / California Sycamore

Secondary Trees:

(Scientific Name / Common Name)

Alnus rhombifolia / White alder

Betula occidentalis / Red birch

Populus fremontii / Fremont cottonwood

Salix gooddingii / Goodding's willow

Juglans californica / California Walnut

Quercus agrifolia / Coast Live Oak

Eucalyptus camaldulensis / Red River Gum

Juglans californica / California Walnut

Shrubs and Groundcovers:

(Scientific Name / Common Name)

Carex spissa / San Diego sedge

Carex tumulicola / Berkeley sedge

Equisetum arvense / Common Horsetail

Juncus patens / Spreading rush

Mimulus cardinalis / Scarlet monkeyflower

Mimulus guttatus / Common monkey-flower

Oenothera elata / Hooker's evening primrose

Polystichum munitum / Western Sword Fern

Ribes speciosum / Fuchsia-flowered gooseberry

Ribes viburnifolium / Island gooseberry

Chapter 3: East Gateway Code

3.4 Building Design and Landscape Standards

TABLE 4B: LANDSCAPE AND PLANT MATERIALS

3.4.6A Site Landscaping

- a. All open space and building setbacks must be landscaped per the requirements of Table 4B.
- b. The landscape should define, unify and enhance the streetscape.
- c. Street trees must be provided to define the street and identify it as a safe environment for automobiles, pedestrians, and cyclists as well as to add beauty and shade to the street. The selection of species must be based upon height, canopy diameter, disease resistance, and transparency (visibility of businesses).
- d. The landscape must be designed to be sensitive to the character, climate, and soils of Santa Paula. Plant materials must be indigenous to the region or similar in character and habitat to indigenous materials. Plant species must be selected that reduce the need for supplemental irrigation water.

3.4.6B Streetscape Landscaping

- a. Along frontages, indigenous trees such as those identified below must be incorporated to provide sustainable habitat and reinforce the intended visual character of the edge of the City. Trees must be tolerant of stress created during periods of drought, and be receptive to the use of ecologically-friendly integrated pest management.
- b. All areas not devoted to paving or buildings must be landscaped and permanently maintained.
- c. A landscape that is complementary to the buildings must be provided adjacent to facades and side elevations as designated in this Section and Urban Standards Section.
- d. Permanent and automatic irrigation facilities must be provided at all landscaped areas.
- e. Landscape and irrigation plan in conformance with these regulations must be submitted to the City of Santa Paula before the City issues building permits.

- f. To minimize exterior water use, the following measures must be incorporated into the design of the public space:
 1. Use of drought resistant species and water conserving native plants;
 2. Low precipitation rate irrigation; and
 3. Use automatically controlled irrigation systems regulated to the actual evapotranspiration rate of the soil.
- g. The landscape must contribute to mitigating environmental degradation resulting from storm water runoff. Methods that reduce runoff and improve water quality must be implemented in landscape design.
- h. Plantings in areas fronting on streets must be appropriate to the scale, orientation and purpose of the area. All trees for front yards when provided must be a minimum of 24 inch box (1" - 1.75" caliper). Appropriate plant materials and designs for specific frontage yard types are as follows:
 - i. At facades and adjacent screenwalls, low shrubs, ground cover, wall vines or any combination thereof, may be planted.
 - j. Lawn may be used in front yard areas, but should be limited to no more than 25% of the entire front yard area.
 - k. In non-front yard areas, Lawn should only be used in areas that it may potentially be used as a play/picnic area.
 - l. Side yards on corners are considered front yards and subject to the same requirements as identified in this section.
 - m. On perimeter streets, large trees must be planted in an informal pattern reflective of the visual character at the City's edge per the East Gateway Specific Plan.
 - n. On interior streets, large trees with arching canopies must be planted as rows in continuous parkway strips or

individual tree wells parallel to the adjacent curb on both sides of the street.

o. Street tree spacing should be between 25 feet and 40 feet on center.

p. Runoff from sidewalks must be conveyed to planted parkways or landscaped setback areas. To the extent possible, overflow from parkways and runoff from the road may be directed into pervious paving in parallel parking areas in order promote infiltration into the ground.

3.4.6C Parking Area Landscaping

a. Parking areas must be landscaped in compliance with the following requirements.

i. Amount of Landscaping. Each parking lot must provide landscaping within and/or around the parking area at a minimum ratio of 10 percent of the gross area of the parking lot. A minimum of one large shade tree must be provided for each fifty parking spaces or trees provided to achieve 50% canopy coverage of paved area at maturity, whichever is greater.

ii. Location of Landscaping. Landscaping must be evenly dispersed throughout each parking area. Orchard-style planting (placement of trees in uniformly-spaced rows) is required for parking areas with more than 20 parking spaces.

b. In parking areas (excluding on-street parking along frontages), indigenous trees such as those identified below must be incorporated to provide sustainable habitat and reinforce the intended visual character of the edge of the City. Trees must be tolerant of stress created during periods of drought, and be receptive to the use of ecologically-friendly integrated pest management.

3.4.7 Standards and Guidelines

Lighting, screening, and service location are subject to the East Gateway Code must comply with the following standards and guidelines, as applicable. Guidelines are distinct from standards through the use of 'should' versus 'must':

1. Each streetscape, building site and parking area must be landscaped per the applicable requirements of Table 4C.

2. Streetscapes, building sites, and parking areas must be in compliance with the following:

3.4.7A Site Lighting and Street Lighting

3.4.7B Building Lighting

3.4.7C Screening of Service and Mechanical

3.4.7D Trash Enclosures

3.4.7E Awnings and Canopies

3.4.7F Bicycle Parking

3.4.7G Shopping Cart Collection Areas

Chapter 3: East Gateway Code

3.4 Building Design and Landscape Standards

TABLE 4C: LIGHTING, SERVICES, UTILITIES

3.4.7A Site Lighting and Street Lighting

Site lighting includes illumination of parking areas, buildings, pedestrian routes, dining areas, design features and public sidewalks or open spaces. For buildings within 10 feet of a public sidewalk or open space, such lighting is identified in the following standards as 'street lighting'.

- a. Site lighting must be shielded by permanent attachments to light fixtures to prevent offsite glare.
- b. The position of a lamp along a sidewalk or other path being illuminated must be within 20 feet of the ground.
- c. Street lighting must be provided along public sidewalks and parking areas through luminaires that provide a minimum of 1 foot candle mounted on 14 to 20 foot tall poles spaced between 50 and 75 feet. Such poles must be at least 18 inches away from the adjacent curb face.

3.4.7B Building Lighting

- a. Lighting fixtures cannot distract from, or obscure important architectural features of the building. Lighting fixtures must be a subordinate feature on the building unless they are incorporated into the overall design scheme of the building.
- b. Lighting (uplighting and downlighting) that is positioned to highlight a building or outdoor artwork must be aimed at the object to be illuminated.
- c. All flood lamps must be shielded so that the light sources are not visible from a public sidewalk, or open space.

3.4.7C Screening of Service and Mechanical

- a. Loading areas cannot be within 35 feet of a primary building facade and must be screened as follows:
 - i. All outdoor storage of materials, products, equipment, or vehicles must be completely screened from both public view and adjacent properties to ensure that outdoor storage does not create any conditions that are detrimental to the appearance of the premises or surrounding properties or in any other manner that is detrimental to the public health, safety, welfare or causes a public nuisance.
 - ii. All walls or fences used for screening purposes must be between 6 and 8 feet tall.
 - iii. When screening walls within 50 feet of a building, the wall must be designed as an extension of the building facade in terms of materials and finishes.
- b. Screening of service entrances must be compatible with the architecture of the adjacent buildings. A wall will be considered compatible if it uses the same material as other buildings on the block, or is in keeping with the color scheme of the adjacent building.
- c. Materials, products or equipment stored outdoors cannot exceed the height of the wall, nor encroach into required parking and landscape areas.
- d. Service areas and mechanical equipment must be visually unobtrusive and integrated with the design of the site and building.
- e. Service entrances, waste disposal areas, openings to a trash enclosure and other similar activities must be incorporated into the building facade per section 3.4.7D 'Trash Enclosures'.
- f. Utility boxes must be positioned to not be seen from a primary building facade. Utility boxes must be located on the sides of buildings and away from pedestrian and vehicular routes or within interior building corners, at building offsets or other similar locations where the building mass acts as a shield from public view.
- g. Air intake and exhaust systems, or other mechanical equipment that generates noise, smoke or odors cannot be located on or within 10 feet of the primary frontage line or along an open space.
- h. HVAC equipment must be located within side yards or in the back half of the building site, away from any street. If necessary to locate the equipment on the roof, the equipment must be incorporated into the design of the roof and cannot face any street.
- i. Storage of goods and supplies must be limited to those allowed to be sold at retail on the premises or utilized in the course of business.
- j. All property must be maintained in a safe, sanitary and attractive condition, including without limitation: structures, landscaping, parking areas, walkways, and trash enclosures.

Chapter 3: East Gateway Code

3.4 Building Design and Landscape Standards

TABLE 4C: LIGHTING, SERVICES, UTILITIES

3.4.7D Trash Enclosures

Trash and recycling containers must comply with the requirements of SPMC 16.40.110. In addition, trash and recycling containers must comply with the following requirements:

- a. Trash and recycling containers must be incorporated into the design of the main building in the form of a trash room.
- b. Trash enclosure doors must be designed a manner that is compatible with the architectural design of the main building in terms of materials, color, and detailing.
- c. Where a service alley is present, trash and recyclable containers and dumpsters must be located and accessed from the alley.
- d. Where a service alley is not present, trash and recyclable containers and dumpsters should be located in a manner that is not visible from surrounding primary streets and/or side streets.

3.4.7E Awnings and Canopies

- a. When awnings and canopies are used they must accentuate the character-defining features of a building.
- b. Awnings and canopies must match the shape of the opening that they are shading except for odd shaped awnings. Odd shapes and bubble awnings are prohibited.
- c. Canopies and awnings cannot conflict with the buildings' proportions or with the shape of the openings that the awning or canopy covers.
- d. Simple shed shaped awnings must be used for rectangular openings.
- e. Awnings and canopies must be mounted to highlight and not obscure architectural features such as moldings above the storefront.
- f. Awnings and canopies may be constructed of metal, wood or fabric, subject to City approval.
- g. Internally illuminated awnings that glow are prohibited.

3.4.7F Bicycle Parking

- a. One bicycle parking stall must be provided for every 50 automobile parking stalls.
- b. Bicycle parking must be distributed to serve all buildings.
- c. Bicycle parking must:
 - i. Be within 50 feet of a building entrance;
 - ii. Be adequately lighted;
 - iii. Provide adequate clearance around the facility to give users room to maneuver.

3.4.7G Shopping Cart Collection Areas

- a. Shopping cart collection areas must be provided in parking areas with more than 50 parking spaces per the following:
 - i. Collection area(s) distributed for each 200 feet of parking spaces;
 - ii. Collection areas must consist of a corral surrounded by a 6 inch curb (without railings or walls) to prevent carts from straying into pedestrian ways and automobile drives;
 - iii. Highly visible signage must be provided to mark a collection area's location.

Chapter 3: East Gateway Code

3.5 Signage Standards

3.5 Signage Standards

3.5.1 Purpose

This section establishes the standards to implement all signage within the East Gateway Code boundaries.

3.5.2 General Standards

The following standards are established and applied to property subject to the East Gateway Code.

1. Property and rights-of-way must comply with all applicable requirements of Section 3.5.

Section 3.5.2A Sign Location Standards

Section 3.5.2B Sign Design Standards

Section 3.5.2C Sign Lighting Standards

Section 3.5.2D Building Identification Standards

Section 3.5.2E Standards for Signage Types (Tables 5A-5G)

2. Number of signs: The maximum number of signs allowed per building is identified for each sign type. Logos or identification symbols are considered signs and must conform to all applicable provisions of Section 3.5.
3. Sign area and height. The maximum sign area and height is identified in the requirements of each allowed sign type in Tables 5A through 5G.

3.5.2A Sign Location Standards

Each sign must be located in compliance with the following requirements and all other applicable provisions of Section 3.5

1. Each sign must be located on the same site as the subject of the sign.
2. Each sign must comply with the building setback and encroachment requirements for the block.
3. No sign must be placed so as to interfere with the operation of a door or window. Signs cannot be located so that they cover architectural features of the building, including, without limitation: transoms, insignias, or any other architectural ornamentation.

3.5.2B Sign Design Standards

In addition to the SPMC requirements, the following design criteria must be used in reviewing the design of individual signs.

1. Signs must be visually consistent with the architectural design and proportions of the building it is attached to.
2. Street address. The City may require that a sign include the street address of the site, where it is determined that public safety and emergency vehicle response would be more effectively served than if the street address were displayed solely on one or more buildings on the site.
3. Permanent signs that advertise continuous sales, special prices, or include phone numbers are not permitted.

3.5.2C Sign Lighting Standards

1. Sign lighting must be designed to minimize light and glare on surrounding rights-of-way and properties.
2. External light sources must be directed and shielded so that they do not produce glare off the site, on any object other than the sign.
3. Colored lights cannot be used at a location or in a manner so as to be confused or construed as traffic control devices.
4. Neither the direct nor reflected light from primary light sources can create hazards for pedestrians or operators of motor vehicles.
5. For energy conservation, light sources must be hard-wired fluorescent or compact fluorescent lamps, or other lighting technology that is of equal or greater energy efficiency. Incandescent lamps are prohibited, except when used in signs of historic character as part of the architectural design.
6. Neon lighting, when allowed to be used, must be exposed, decorative neon tubing in front of a decorative sign in compliance with the applicable signage type standards.

3.5.2D Building Identification Standards

1. Building identification signs must comply with the following standards:
 - a. Building identification signs cannot be taller than 12 inches and not longer than 20% of the primary facade length. Such signs must be designed and sized proportionately to the building being identified and to the sign area in which it is located.
 - b. Building identification signs may contain the name of a building, or describe its function or tenant, but cannot identify any products sold.
 - c. Building identification signs may be made of illuminated individual letters applied to the building face, may be engraved into the building's material, or may be low-relief.

2. A directory sign identifying businesses above the ground level must comply with the following:
 - a. Must be located at the ground level and adjacent to the entry point for the upper levels.
 - b. No more than one directory sign per ground level entrance.
 - c. The area of the directory sign cannot exceed six square feet.
 - d. The directory sign may only identify the names of businesses above the ground level.
 - e. Letter height cannot exceed two inches.

3. Service entry wall signs must comply with the following.
 - a. Must be located adjacent to the ground level service entrance.
 - b. No more than one service entry wall sign per facade.
 - c. The area of the service entry sign cannot exceed four square feet.

3.5.2E Standards for Signage Types

Signage within the East Gateway Code boundaries must be designed and maintained as any of the following sign types, as allowed by the overlays identified in Figure 2:

Table 5A Standards for Wall Signs

Table 5B Standards for Window Signs

Table 5C Standards for Awning / Canopy Signs

Table 5D Standards for Projecting Signs

Table 5E Standards for Monument Signs

Table 5F Standards for Highway Signs

Table 5G Standards for Roof Signs

3.5.3 Required Findings

Applications involving Section 3.5 of the East Gateway Code must be consistent with the applicable findings below in order to be approved:

"The proposed application..."

- a. Generates or maintains the intended streetscape through the design of signage which is contextually appropriate to the building and site arrangement;

- b. Is consistent with the intent of the applicable frontage zone along which the building(s) is located;

- c. Is in compliance with the applicable requirements of Section 3.5 and the applicable signage type(s).

Chapter 3: East Gateway Code

3.5 Signage Standards

A sign that is painted or applied directly to the wall, typically above the storefront or more creatively as approved by the City. This type consists of a single externally illuminated panel or individual letters and/or logo and does not include cabinet signs. This type of sign is intended for viewing from across the street and along the sidewalk.



Example - Individual, internally illuminated letters mounted directly on wall.



Example - Letters mounted directly on wall with decorative external lamps



Diagram - See Design Standards below for requirements.

1. Max 1 wall sign per business within a facade that faces a sidewalk or plaza;
2. Max thickness of sign as measured from the wall cannot exceed 4 inches;
3. Min 24 inches between sign and any opening or edge of a facade;
4. Min 12 inches between sign and an eave or parapet;
5. If illuminated, external illumination required and must be mounted to maintain visual integrity of the sign. Neon may be used only when exposed, decorative, and in front of a decorative frame or background.

	WALL SIGN	Min.	Max.
a	Height	n.a.	24"
b	Width (as % of facade width)	n.a.	60%
c	Feature (allowed beyond sign area)	n.a.	6 sq ft

Awning/canopy, projecting, window, subject to city review and approval

A sign that is painted or applied directly to the storefront window(s) and/or door(s). This type consists of individual letters and a logo with allowances for some contrasting background. This type of sign is intended for viewing from across the street and at close range.



Example - Individual vinyl letters with feature area at top center and contrasting background along bottom.



Example - Individual letters painted directly on inside of window.



Diagram - See Design Standards below for requirements.

1. Max 1 window sign per storefront within a facade that faces a sidewalk or plaza;
2. Min 75% of sign must be independent of contrasting background;
3. Signage inside the shop space is not allowed within 4 feet of the window;
4. Storefront width is measured between walls or columns of at least 8" in width.

	WINDOW SIGN	Min.	Max.
a	Height	n.a.	12"
b	Width (as % of storefront width)	n.a.	40%
c	Contrasting background (as % of sign area)	n.a.	25%
d	Feature (allowed beyond sign area)	n.a.	2 sq ft

Awning/canopy, projecting, wall, subject to city review and approval

Chapter 3: East Gateway Code

3.5 Signage Standards

A sign that is integral to the awning or canopy above a storefront. This type of sign can be located on the valence face including feature area on the main panel, or in the case of a canopy, on top of or below and along the front edge of the canopy. This type of sign is intended for viewing at close range.



Diagram - See Design Standards below for requirements.

1. Max 1 wall sign per awning or canopy that pertains to a storefront facing a sidewalk or plaza;
2. Awning / Canopy must correspond to a storefront per applicable requirements of Section 3.4.7E;
3. The sloped panel may only be used for logo feature and cannot be for additional signage area;
4. Internal illumination of awning(s) is prohibited;
5. Canopy sign may extend above the top or below the canopy provided that the sign does not exceed 24" in height and a minimum of 8 feet clear is provided between the sidewalk and the bottom of the sign;

AWNING / CANOPY SIGN		Min.	Max.
a	Height: awning valance	6"	12"
b	Height: canopy valance	n.a.	24"
c	Width (as % of awning/canopy width)	n.a.	75%
d	Feature (logo) for awning main panel	n.a.	2 sq ft
e	Feature (allowed beyond canopy sign area)	n.a.	3 sq ft
f	Vertical clearance from sidewalk	8'	n.a.

Projecting, wall, window, subject to city review and approval



Example - Sign on valance with logo on main panel.



Example - Painted panel hanging from gallery frontage

A two-sided sign that projects over a sidewalk, courtyard, dining patio or other such open space. This type of sign is intended for viewing at close range.



Example - Projecting sign with decorative bracket.



Example - Rectangular shape with stylized edge..



Diagram - See Design Standards below for requirements.

1. Max 1 wall sign per business within a facade that faces a sidewalk or plaza;
2. If illuminated, external illumination required and must be mounted to maintain visual integrity of the sign. Neon may be used only when decorative, exposed, and in front of the background of the projecting sign.
3. Supporting hardware such as brackets must be architecturally compatible with the building facade;
4. Minimum 5 feet separation from another projecting sign or awning;

PROJECTING SIGN		Min.	Max.
a	Height	n.a.	18"
b	Width	n.a.	36"
c	Thickness	n.a.	3"
d	Feature (allowed beyond sign area)	n.a.	1.5 sq ft
e	Vertical clearance from adjacent s.w.	8'	12'
f	Horizontal clearance from adjacent curb	24"	n.a.

Awning/canopy, wall, window, subject to city review and approval

Chapter 3: East Gateway Code

3.5 Signage Standards

A sign that is located in a large front yard setback between a building and the sidewalk. This type of sign typically occurs as part of site-defining landscape in the form of a low wall that encloses outdoor dining or open space areas. This type of sign may include a gateway that provides entry to the space in front of the building and to the building itself.



Diagram - See Design Standards below for requirements.



Wall-integral example - Individual letters attached to stone wall highlighted by appropriate landscape.



1. One sign may be placed at each end of the wall or on each street-facing side of the wall provided that there is at least 50 feet between each sign.
2. The wall may include a gateway of at least 5 and up to 8 feet wide and at least 8 and up to 12 feet tall. A small hanging sign may be installed in the gateway if no larger than 18 x 24 inches and at least 8 feet vertical clearance is provided.
3. Not allowed to encroach into any required visibility area, right-of-way, or private street;
4. Colors and materials must match and complement the building style and details.
5. Lighting must be from external or ambient sources.

	MONUMENT SIGN	Min.	Max.
a	Height	none	24"
b	Width	none	15'
c	Thickness of wall	n.a.	18"
d	Feature (allowed beyond sign area)	n.a.	2 sq ft
e	Horizontal clearance from adjacent curb to be landscaped	5'	n.a.
f	Gateway	min 8' vertical clearance min 5' horizontal clearance	

Awning/canopy, projecting, wall, window, subject to city review and approval

A vertically-oriented sign that is detached from any building, located along the primary frontage, consisting of exposed structural supports with multiple signs mounted on each of its two sides. This type of sign is intended for viewing from a distance to identify the shopping center or a business set back and not readily visible from the street.



Diagram - See Design Standards below for requirements.

1. Minimum of 1,000 linear feet of frontage along Hwy 126. Maximum of 1 highway sign per Specific Plan boundaries.
2. Maximum of six businesses (not including the name of the shopping center) to be identified on the sign;
3. Artwork that is relevant to Santa Paula such as packing crate labels or other City images may be included.
4. The overall structure may have up to two sides--one to face each direction along the Heritage Valley Highway frontage;
5. Sign, including overhead structure cannot encroach into any right-of-way;
6. Supporting hardware such as brackets must be architecturally compatible with the architectural style of the adjacent buildings

HIGHWAY SIGN		Min (ft)	Max (ft)
a	Overall Height	25	55
b	Overall Width	20	35
c	Tenant Sign Area height from grade	no min	50
d	Distance from Sign Area from grade	15	35
e	Size of Shopping Center Name	4 x 20	8 x 20
f1	Tenant Sign Area Overall	no min	20 W x 30 T
f2	City Image Sign Area Overall	no min	15 W x 30 T
g	Exposed Structure without Signage	10	25

Chapter 3: East Gateway Code

3.5 Signage Standards

A sign that is at the edge of Santa Paula and serves as a landmark. This type of sign is intended to help emphasize the identity and presence of the community as a whole and therefore, the signage is allowed to be more inventive.



Diagram - See Design Standards below for requirements.

1. Max 1 sign per building in compliance with Section 'E' 'Height Limit' for applicable block. Sign must be designed integrally with the roof of the building;
2. Building must be within 1000 feet of east City boundary;
3. Minimum separation of 800 feet from another such sign;
4. Structure supporting the sign must be integral to the design and architectural style of the building;
5. Lighting must be exposed neon or decorative external sources as approved by the City;
6. Colors and materials must complement those of the building.

	ROOF SIGN	Min.	Max.
a	Height	5'	15'
b	Length	15'	75% of ridge length
c	Thickness	none	12"
d	Feature (allowed beyond sign area)	none	25 square feet

Awning/canopy, projecting, wall, window, subject to city review and approval



Examples - Large, district-wide type of signs aimed at advertising the district as much as the individual business.

Key

- LW Logo Width
- LH Logo Height
- SW Sign Width
- SAW Sign Area Width
- SH Sign Height
- SAH Sign Area Height
- FAW Feature Area Width
- FAH Feature Area Height



4.1 Purpose and Intent

The intent of the infrastructure plan is to assure the Specific Plan site is developed in such a way as to support the intensity of proposed land uses, and that on-site and off-site infrastructure systems have adequate capacity to serve the project at build out. The sewer and water master plans for the project are based on the City of Santa Paula Wastewater and Water Master Plans, which were both updated in June 2012, and are in conformance with these plans. The drainage master plan is based on the City of Santa Paula Drainage Master Plan and is in conformance with this plan.

The development of the Santa Paula East Gateway Specific Plan will require extending infrastructure and providing services into the Specific Plan area. Basic utilities, including, water, sewer, and other utilities exist in locations adjacent to the site. The Government Code requires a specific plan to include text and diagrams that specify: "The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan."

This section of the Santa Paula East Gateway Specific Plan will fulfill this requirement and meets the Santa Paula Municipal Code requirements for preparing specific plans. In addition, this section facilitates orderly development by identifying "backbone" utility infrastructure needed to support the proposed development.

Chapter 4: Infrastructure

4.2 Grading

4.2.1 Conceptual Grading Plan

Grading for the project is depicted on the Conceptual Grading Plan for the Gateway Specific Plan, as shown on Figure 4-1. The existing site is approximately four feet lower than Highway 126 and slopes southerly towards Santa Clara River. The site elevations will be raised to match Highway 126 elevations along the north property line, to elevate the site above the Base Flood Elevation for Orcutt Creek, and to maintain cover over the future sewer system. The total fill required at the time of grading permit issuance to raise the site is between 110,000 and 150,000 cubic yards. A Conditional Letter of Map Revision (CLOMR) will be required to remove this site from the floodplain created by Orcutt Creek. The importation of fill material to the site may occur in a phased manner over the build-out period of the project.

Figure 4-1: Conceptual Grading Plan



4.3 Drainage

4.3.1 Conceptual Drainage Plan

Drainage for the Santa Paula East Gateway Specific Plan is presented in Figure 4-2. Storm drain facilities shall be sized to meet City of Santa Paula standards. The storm drain system will discharge at the southeastern edge of the property into the existing channel that parallels Orcutt Creek. Stormwater conveyance and treatment requirements shall meet City of Santa Paula and State Water Resources Control Board MS4 Requirements.

Treatment systems will be based on the treatment volume calculation guidelines provided in the approved Ventura County Water Quality Manual at the time of Specific Plan approval. The treatment types will include bioswales, bioretention cells, infiltration trenches, permeable pavement and/or detention basins as needed based on the proposed site plan layout.

Variations of this backbone stormwater system for alternative plan layouts are shown in Figures 4-3 and 4-4 to the right.

Figure 4-2: Conceptual Drainage Plan (Alternative A)

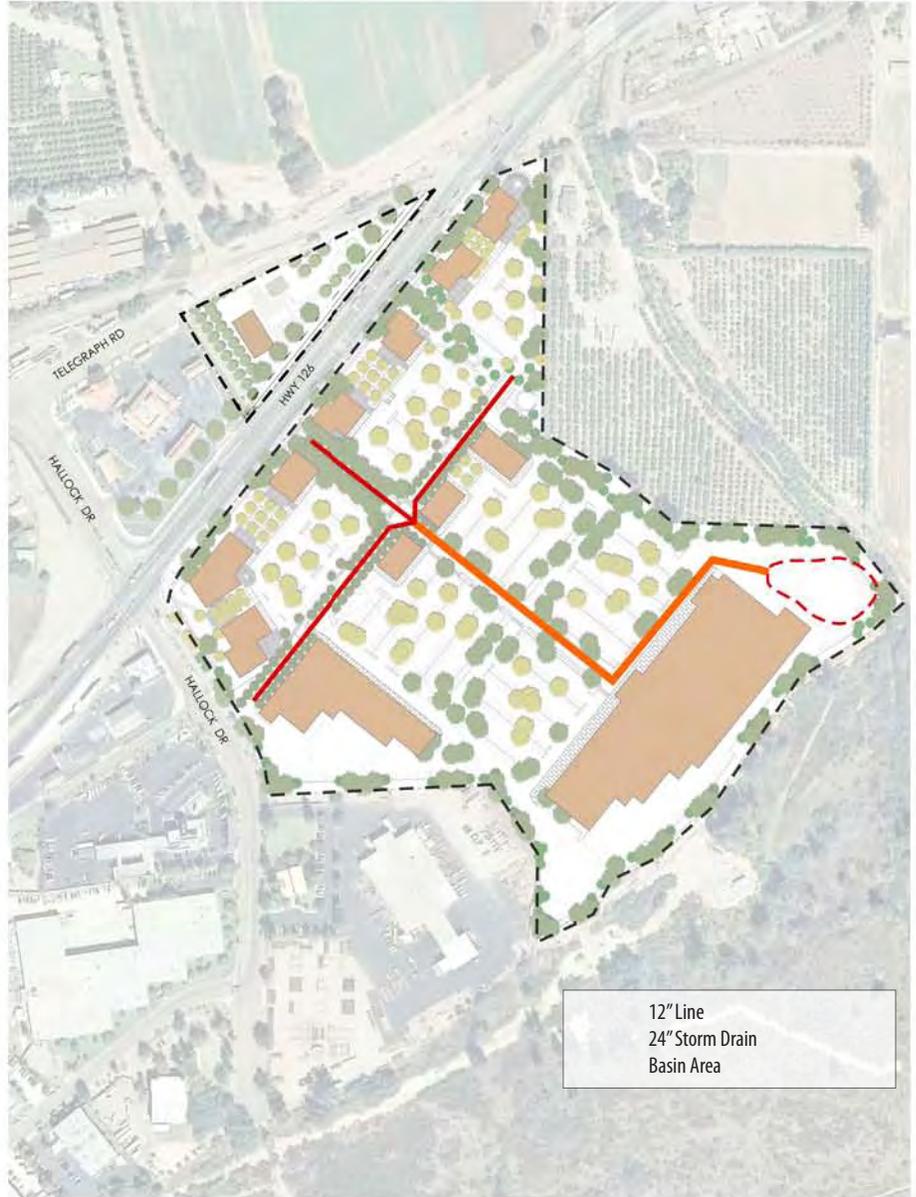


Figure 4-3: Conceptual Drainage Plan (Alternative B)



Figure 4-4: Conceptual Drainage Plan (Alternative C)



Chapter 4: Infrastructure

4.4 Water

4.4.1 Conceptual Water Plan

Water supply to the East Gateway Specific Plan project site is currently supplied by the City of Santa Paula. Existing services that cross the site from the north will be re-routed to accommodate the eventual development plan. Three farmers' irrigation wells (shown as FICO wells in Figures 4-5, 4-6, and 4-7) exist along the southern property line. These wells and their service lines will be included in the final development plan so they can remain active.

On-site water lines are sized in accordance with calculations for the worst case water demand. These water demand values were obtained from the City of Santa Paula's 2005 Potable Water System Master Plan and the amendment dated June, 2012. Based on these factors, the average water demand for the proposed East Gateway specific plan is approximately 65 AFY. The demand estimates uses a value for commercial demand of 2.0 AF/ac/yr.

A preliminary water layout for the Santa Paula East Gateway Specific Plan is presented in Figure 4-5. The project will be served from the existing City of Santa Paula water main in Hallock Drive to the west, and the new system installed in East Area 1 to the north. The East Gateway Project would be serviced by either the 200 or 300 pressure zone and will be determined at the time of the new water system construction. This looped water system provides adequate water pressure and fire flow for proposed structures. The East Area 1 Specific Plan project is conditioned to provide a 3.0 million gallon storage tank, which will serve a new 300 Zone domestic system, a 2.0 million gallon tank for City uses is also proposed, however the 3.0 million gallon tank has capacity for both East Area 1 and the East Gateway. projects. This will ensure the fire protection capacity of the proposed system. Project demand is outlined in the table to the right. Variations of this backbone

Figure 4-5: Conceptual Water Plan (Alternative A)

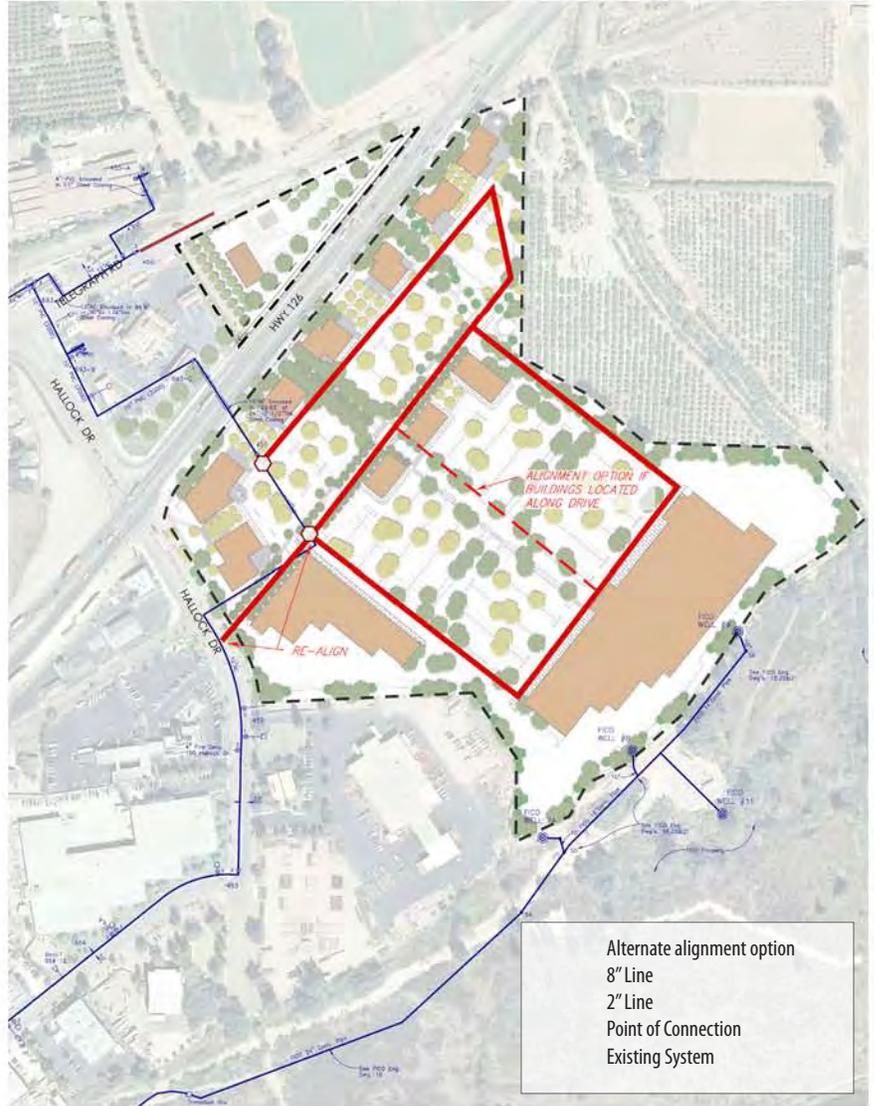


Table 4.4.1: Project Demand

32.3	2.0	65
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Notes

- ^[1] Flows calculated based on gross acreage of Development Area.
- ^[2] Refer to City of Santa Paula Water Master Plan. Water demand for industrial and commercial uses is based on acreage.
- ^[3] Refer to City of Santa Paula Water Master Plan. Commercial demand = 15gal/SF/yr.

Figure 4-6: Conceptual Water Plan (Alternative B)

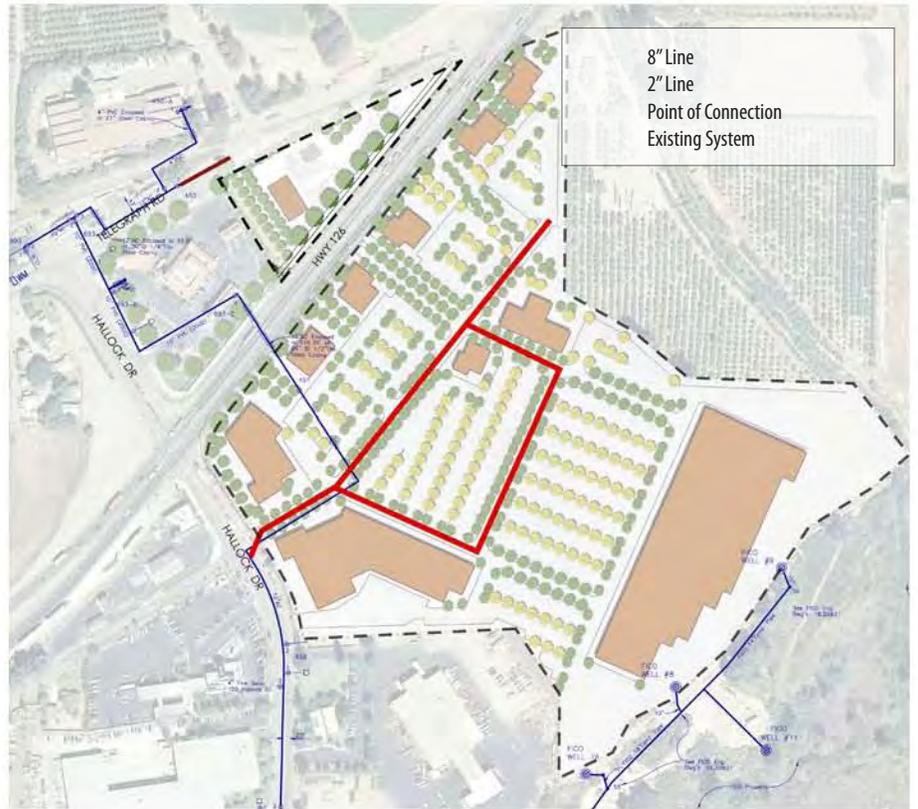


Figure 4-7: Conceptual Water Plan (Alternative C)



4.5 Recycled Water

4.5.1 Conceptual Recycled Water Plan

Currently there are no recycled water systems in the East Gateway project vicinity. The Gateway recycled water system would operate via a proposed 12" distribution main as called for by the City's Recycled Water Plan and per East Area One's specific plan. This main will be available after the City's water recycling plant is operational. The main will terminate at the intersection of Hallock Drive and Telegraph Road, which will become the point of connection for the East Gateway Project. A preliminary recycled water site plan layout is presented in Figure 4-8. The project will be serviced with an 8" main for the purposes of irrigation for planters and open space. Variations of this backbone stormwater system for alternative plan layouts are shown in Figures 4-9 and 4-10 to the right.

Figure 4-8: Conceptual Recycled Water Plan (Alternative A)

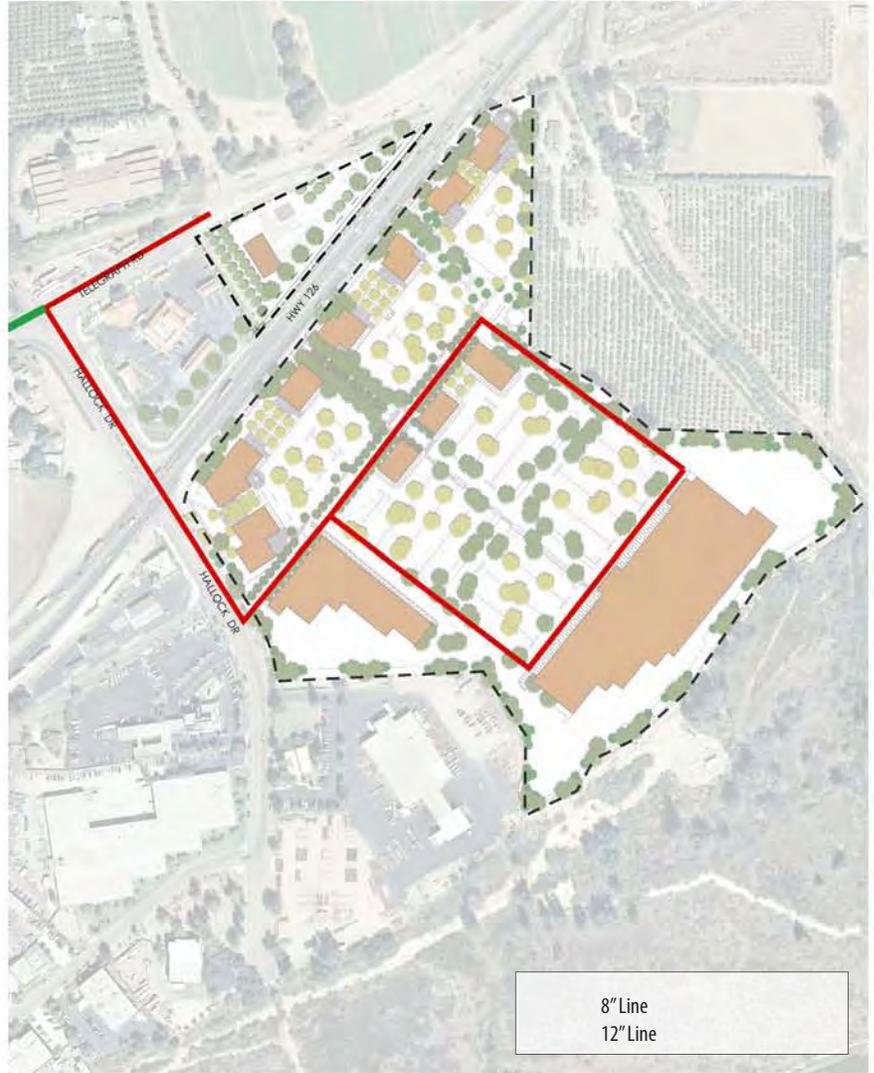


Table 4.4.1: Project Demand

32.3	2.0	65
------	-----	----

Notes

^[1] Flows calculated based on gross acreage of Development Area.

^[2] Refer to City of Santa Paula Water Master Plan. Water demand for industrial and commercial uses is based on acreage.

^[3] Refer to City of Santa Paula Water Master Plan. Commercial demand = 15gal/SF/yr.

Figure 4-9: Conceptual Recycled Water Plan (Alternative B)



Figure 4-10: Conceptual Recycled Water Plan (Alternative C)



4.6 Sewer

4.6.1 Conceptual Sewer Plan

The Gateway Specific Plan area does not have an existing sewer service, However the sewer services from north of Highway 126 run through the property and head west into Hallock Drive. These existing services will be maintained, but will be re-routed to accommodate the site plan. A preliminary sewer site plan layout for the Santa Paula East Gateway Specific Plan is presented in Figure 4-11. The new system will connect to the existing sewer main in Hallock Drive at two separate locations. The first will be at the proposed project entry on Hallock Drive and the second will be approximately 250 feet downstream. The existing 8" main line from the point of connection south to the existing sewer manhole at the intersection of Hallock and Lemonwood Drive will be upgraded to a 12" main to lessen the slope in the street and maintain capacity. A new lift station will be constructed with the development of East Area One, downstream of the project's point of connection to the City System on Lemonwood Drive per the latest City of Santa Paula Wastewater Master Plan. This lift station will be sized to handle the additional flows generated by the Gateway Specific Plan. Sewer lines are sized in accordance with a sewer study for the property's intended use. Wastewater generation for the Gateway Specific Plan at buildout is estimated at approximately 0.11 million gallons per day (MGD). See table below.

Variations of this backbone stormwater system for alternative plan layouts are shown in Figures 4-12 and 4-13 to the right.

Figure 4-11: Conceptual Sewer Plan (Alternative A)

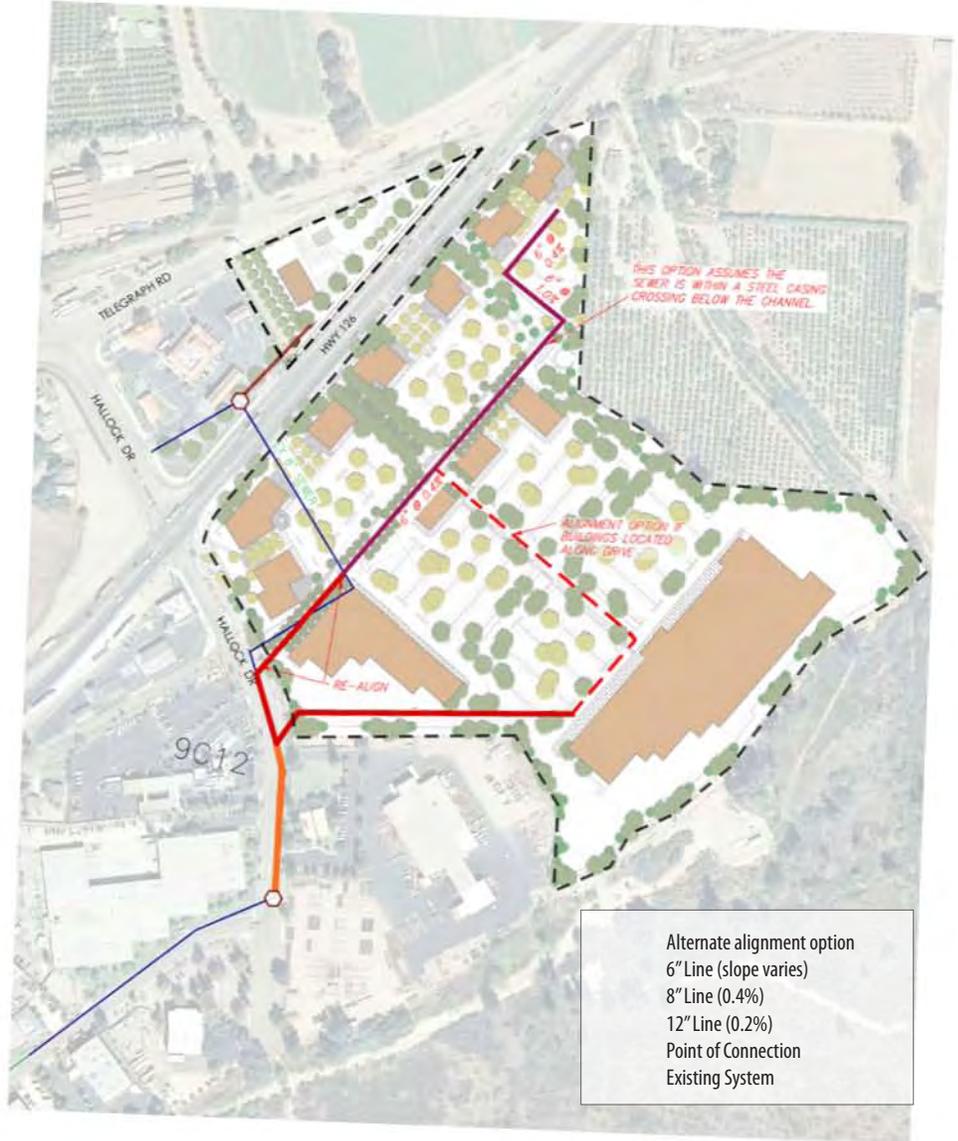


Table 4.6.1: Wastewater Generation

32.3	2.0	0.93 gpm/ AC ^{[1][2]}	30.04	75.10	0.067	0.167	0.108
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Notes

- [1] Commercial sewer generation rate is 75% of the water demand rate based on Section 3 of the City of Santa Paula Wastewater Master Plan.
- [2] Generation Rate conversion from 2 AF/ac/yr to gpm/AC = 0.93.
- [3] Flows calculated based on gross acreage of Development Area per City of Santa Paula Master Plan.
- [4] Refer to City of Santa Paula Water Master Plan. Commercial demand = 15gal/SF/yr.

Figure 4-12: Conceptual Sewer Plan (Alternative B)



Figure 4-13: Conceptual Sewer Plan (Alternative C)



Chapter 4: Infrastructure

4.7 Public Services and Utilities

4.7.1 Public Services

Schools

The Santa Paula East Gateway Specific Plan is intended for commercial and business uses, and therefore will not generate additional students, which are served by the Santa Paula Unified School District. The East Area 1 Specific Plan north of Hwy 126, however, includes school sites for primary, secondary, and postsecondary educational facilities.

Fire Protection

Fire Protection Services for the project area will be provided by the Santa Paula City Department (SPFD). The SPFD provides the City with fire prevention, rescue, and basic emergency medical services: hazardous materials mitigation; and disaster planning coordination.

The Santa Paula Fire Department has two fire stations located on the East and West sides of town. Fire Station 81 is located on the Eastside at 114S. 10th Street. Fire Station 82 is located on the Westside at 536 W. Main Street (refer to figure 3-10) Fire Station 81 is staffed 24 hours a day with (3) three full-time firefighters, while Station 82 is staffed with a combination of (1) full-time captain supervising (2) full-time firefighters.

The majority of the Santa Paula East Gateway Specific Plan area is designated in the “low range area” of the Fire Hazard Zones. The City’s goal is to keep the Fire Departments response time to five minutes or less. The closest fire station to the Specific Plan area is Fire Station #81, with an average fire suppression response time of approximately 4-5 minutes.

A Fire Station site is proposed within the East Area 1 Specific Plan, directly adjacent on the north site of Hwy. 126. In addition to reserving the acreage, the Developer must pay the City’s Fire Suppression Facilities Fee.

Police Services

The Project would be served by the Santa Paula Police Department (SPPD), which currently operates out of a 4,728

square foot facility on 10th Street and the 650 square foot Las Piedras Park Community Policing Building. A two acre parcel for Police and Fire Department usage will be reserved within the adjacent East Area 1 Specific Plan in accordance with that projects’ approval.

Solid Waste

Solid waste pickup, recycling and disposal services will be provided by the City of Santa Paula via a contract with Crown Disposal.

The City also participates in a curb side recycling program, which includes the recycling of glass (food and beverage containers), metal (aluminum cans, etc.) and plastic. The City also provides curbside pickup of paper, cardboard, and yard trimmings, as well as community drop off events for residents to dispose of large items, household hazardous waste, and motor oil and filters.

The Santa Paula East Gateway Specific Plan includes policies that support recycling to reduce the amount of solid waste sent to the landfill. Waste carts for trash, recycling, and green waste will be provided.

4.7.2 Public Utilities

Electricity

Electricity in Santa Paula is supplied by Southern California Edison Company (SCE). High Voltage, 66 kilovolt, transmission lines exist crossing Ojai Road (SR 150), along a portion of 12th Street south of Orchard Street, and along the south side of the rail road tracks, east of 12th Street. A Southern California Edison substation, (the “Wakefield Substation”), is located south of the railroad tracks at the intersection of 12th Street and the railroad tracks. SCE will service and maintain the project area’s electrical facilities. New local serving electrical lines will be placed underground, with conduits in full encasement. The Santa Paula East Gateway Specific Plan includes energy conservation related design standards to reduce electric energy consumption.

Gas

The Southern California Gas Company (SCG) provides natural gas in Santa Paula. SCG serves much of Southern California with a network of transmission and distribution lines. An existing 12 inch high-pressure supply line runs east-west in Telegraph Avenue (SR126), and there is existing gas service on South Hallock Drive and Lemonwood Ave. These in turn branch into a network of smaller gas mains in all of the streets. Service connections will be provided and maintained throughout the Specific Plan area as needed.

Telephone

Telephone service and maintenance to the area is provided by Verizon. Telephone facilities will be located underground within the streets rights-of-way. No overhead telephone facilities will be permitted.

Cable

Cable television is provided in the area by Time Warner. This company will serve the East Gateway Specific Plan area. Cable television facilities will be located underground within public rights-of-way.

5. Implementation

5.1 Introduction

5.1 Introduction

This section responds to Government Code §65451 which requires that specific plans include a program for implementation including regulations, conditions, programs and additional measures necessary to implement the plan.

The responsibilities and procedures required for implementing the East Gateway Specific Plan are identified in this section, including responsibility for capital improvements and financing, and the regulations that will govern its implementation.

Public and capital facilities within and adjacent to the Plan Area will be required to support its development, including water and sewer systems, new roadways, dry utilities, and drainage. The developer of Plan Area improvements will finance and construct all such improvements - along with all private on-site improvements - and will pay all required public agency fees, including fees for law enforcement, fire protection, traffic, sewer, storm drainage, community facilities, parks and schools.

5.2 Specific Plan Regulatory Approach

The implementation procedures set forth in this section are intended to ensure the development of East Gateway in accordance with the planning and design intent of this Specific Plan, and all applicable City of Santa Paula regulations.

The East Gateway Specific Plan applies to all lands within the boundaries shown in Figure 1.4. All development proposals within the Specific Plan boundaries must be consistent with this Specific Plan and the Santa Paula General Plan. The Development Standards, presented in Section 3 of this Specific Plan, contain development regulations which upon approval of this Specific Plan by the City Council will become mandatory in authority for all properties within the Specific Plan boundaries.

5.3 Implementation Schedule

It is anticipated that development of the East Gateway Specific Plan will occur in one or more phases over a two (2) to five (5) year period of time, beginning within five (5) years of the adoption of this Plan. With this time frame in mind, and in order to coordinate infrastructure and financing needs, the phasing of the Specific Plan area is designed to meet the following objectives:

- Orderly build-out based upon market and economic conditions.
- Providing adequate infrastructure and public facilities concurrent with development of each phase.
- Protection of public health, safety and welfare.

The infrastructure improvements, as determined by the City Engineer and per the Finance Plan in this section, will be matched to meet the needs of each phase of development.

5.4 Infrastructure and Public Facilities

This section provides an overview of the parties involved in the implementation, ownership, and long-term maintenance responsibilities for the infrastructure and public facilities required to support the Specific Plan area. It is assumed that some of these improvements and services will be financed through dedicated fees and rates.

5.5 Financing Plan

The financing and maintenance plan for the East Gateway Specific Plan will ensure the timely completion of public facilities, streets, utilities, and other necessary capital improvements, as well as the proper maintenance of these facilities. The developer(s) of the East Gateway Plan Area will fund these, according to the following principles:

- Fully fund new on-site and off-site public infrastructure and services needed to support the Specific Plan development;
- Utilize existing fee programs to fund required off-site infrastructure.
- Fund the costs of mitigating identified adverse impacts that would result from this project on the City's existing infrastructure;
- Phase on-site improvements to ensure they are constructed when necessary and when funds are available to construct public improvements;
- Provide for reimbursements from other development for infrastructure costs that the Specific Plan area is required to advance.

The following policies shall govern the financing of infrastructure and public services for the East Gateway Specific Plan in accordance with the above principles:

- Require development projects within the Specific Plan boundaries area to fund the over-sizing of facilities if required by the City, subject to reimbursement from future developments benefiting from the over-sizing.
- Require dedication of land for road improvements and construction of road improvements consistent with city-wide policies.

If determined necessary by the City, the Master Developer will prepare a detailed financing plan that will specify the financing strategy in greater detail and provide additional infrastructure financing options, such as bonds secured by special taxes.

5.6 Methods and Procedures for Implementation

The procedures, regulations, standards and specifications described in this Specific Plan supersede any conflicting portions of the SPMC. Any development regulation and building requirement not addressed in this Specific Plan is subject to the SPMC, adopted ordinances and resolutions, and all applicable City policies and procedures.

5.7 Specific Plan Approvals and Amendments

There are four levels of action for the East Gateway Specific Plan by the City of Santa Paula. The first level is adoption of the Specific Plan along with annexation of the easterly 25 acres of the site into the City of Santa Paula. The second level rezones the entire Specific Plan area from its current zoning to East Gateway Specific Plan. The third level implements the development allowed by the plan through approval of a Tentative Tract Map identifying the public improvements and the parcels for development. The fourth level is through Site Plan Review for individual development projects per the requirements of this Specific Plan. In addition, certain land uses, as identified in Table 2, require a Conditional Use Permit. Such land uses are subject to the review and approval of the Planning Commission in a public hearing.

5.7.1 Specific Plan Adoption

The East Gateway Specific Plan shall be adopted by ordinance of the Santa Paula City Council upon consideration of the Planning Commission’s review and recommendation. Thereafter, upon annexation of the unincorporated 25 acres of the site to the City of Santa Paula’s jurisdiction, the land uses and regulations of this Specific Plan will be mandatory for development in the Specific Plan area.

5.7.2 Zone Map Amendment

The various land uses and regulations of this Specific Plan are compatible with the objectives, policies, general land uses and programs specified in the Santa Paula General Plan. In order to implement this Specific Plan, the existing zoning of Commercial Highway and Commercial Light Industrial will be replaced by the East Gateway Specific Plan designation.

5.7.3 Tentative and Final Subdivision Maps

Prior to the construction of any improvements within the Plan Area, a Tentative Tract Map (Map) meeting all requirements of the SPMC and the California Subdivision Map Act shall be prepared for the entire Plan Area. The Map shall be submitted, reviewed and approved in accordance with the SPMC and the California Subdivision Map Act. For projects requiring a tentative or parcel map(s), the provisions and procedures of this Specific Plan apply, unless otherwise provided for in the SPMC. A tentative map or parcel map may be processed concurrently with this Specific Plan. Applications for tentative and final subdivision maps, parcel maps, and lot line adjustments must be filed with the Planning Director in accordance with the SPMC. Elements and systems depicted on the Map shall include, but are not limited to:

- All proposed right-of-way dedications and/or easements for public access and utilities.
- The alignments of all private streets and drives - and thus the pattern and boundaries of on-site blocks - along with the general conceptual layout of buildings and parking lots, for reference. This Specific Plan provides a range of alternative block configurations to support market-based decision making as Plan implementation proceeds. At the time of Tentative Map preparation, one of those

5. Implementation

5.6 Specific Plan Approvals and Amendments

alternatives must be selected, and the block structure and dimensions defined.

- All proposed vehicular and pedestrian points of access, ingress and egress to and from the blocks of the Plan Area.
- The general layout of public and private utility systems within the Plan Area, any required off-site utility improvements, and points of connection.
- The conceptual grading of the Plan Area, including the limits of such grading in relation to existing topography on and adjacent to the Plan Area.
- Proposed phasing of improvements, if any.

5.7.4 Master Site Plan Review and Approval

Concurrently and in coordination with Tentative Tract Map preparation, a Conceptual Master Site Plan shall be prepared and submitted to the City, depicting the general locations and sizes of buildings, and the general character of on-site and off-site landscaping.

If infrastructure improvements and building and site improvements are proposed to be constructed in a single phase, the Master Site Plan will serve as the site plan for that single project and will be reviewed and approved along with the development and design review process for that single project.

If the Plan Area is proposed for construction in multiple phases, such phasing will be approved and conditioned through the Tentative Map process, and the Master Site Plan will serve to coordinate the phased development of the blocks and buildings of the Plan Area over time. Development Plans and Final Maps for each Phase shall be found to be consistent with the Tentative Map and Master Site Plan. Whether developed in one phase or multiple phases, the Development Plan Review process will be as follows.

5.7.5 Development Plan Review

Design Review for development within the Specific Plan boundaries will occur at two (2) levels. First, the Master Developer will review all applications for development by Developer(s). Following review and approval by the Master

Developer of individual applications, application for Site Plan Review will be made to the City of Santa Paula.

Before the City issues any commercial, industrial or institutional building permits for any specific phase, a Development Plan must be approved for such development. This may be a single Development Plan for the entire Plan Area, or multiple Development Plans for multiple phases of development.

The primary purpose of the Design Review, as set forth in SPMC Chapter 16.226, is to determine that individual projects are being developed in a manner that conforms with the goals and standards specified by this Specific Plan and to ensure compatibility with all appropriate City regulations. The Design Review Committee has the authority to approve, amend or deny development plans per the intent of the development standards of this Specific Plan.

The Committee will meet to review development plans within fifteen business days after a complete application is provided for review. Decisions of the Committee may be appealed to the Planning Commission within 15 business days after the Committee determination. The Planning Commission, within 40 days of reviewing said appeal, can affirm, reverse, or modify the decision of the Development Plan Review Committee. The Planning Commission decision may be appealed to the City Council by following the same procedure set forth above for appeal to the Planning Commission.

5.7.6 Maximum Development Yield

The maximum development yield for all of the East Gateway Specific Plan area is 350,000 square feet of commercial or industrial space as allowed by this Specific Plan.

5.7.7 Amendments to the Specific Plan

Any proposed changes to this Specific Plan that would substantially alter the Land Use Plan contained within this document are considered amendments to the Specific Plan and must be processed pursuant to provisions contained in Government Code § 65453 and in the same manner as a zoning text amendment.

It is anticipated that as implementation occurs over the life of this project, certain modifications to the Specific Plan text and exhibits may be necessary during the life of the project. Any modifications to the Specific Plan must occur in accordance with the amendment process described in this section.

Minor modifications allow for changes to the Specific Plan that do not conflict with the intent of the Specific Plan and are focused in scope so as not to require a formal review by the Planning Commission. Requests for minor modifications that are found to be in substantial compliance with this Specific Plan may be approved by the Planning Director. All other proposed changes are considered formal amendments and are subject to the review and approval of the Planning Commission and City Council per SPMC §16.216.080. In all cases amendments must be found to be consistent with the goals and policies of this Specific Plan, and the Santa Paula General Plan.

5.7.7.1 Minor Modifications

Upon determination by the Planning Director, certain minor modifications to the Specific Plan text and/or graphics may not require a formal amendment (i.e. through public hearing) to the Specific Plan as approved. The Planning Director has the authority to approve modifications to the Specific Plan as follows:

1. Realignment or modifications to internal streets serving the project, lot lines, easement locations and grading adjustments, if approved by the City Engineer, subject to Chapter 3 of the East Gateway Code.
2. Minor modification to design criteria such as paving treatments, architectural details and related criteria.
3. Minor modification to landscape treatments, fencing, lighting, trails, and entry treatments, provided the modifications are in substantial conformance with the purpose and intent of the specified design criteria.

5.7.7.2 Formal Amendments

All Specific Plan modifications, which do not meet the criteria of an Administrative Modification noted in this section, require Formal Amendment of the Specific Plan. Such amendments must be processed pursuant to SPMC §16.216.080 and are subject to the review and approval of the Planning Commission and City Council.

The Specific Plan text and exhibits represent an integrated, well-balanced plan for development, which has been reviewed in great detail by the City's staff, Planning Commission and City Council. Therefore, it is the applicant's responsibility to demonstrate that:

1. The proposed amendment meets the goals and objectives of the Santa Paula General Plan; and
2. Any impacts to this Specific Plan resulting from the amendment can be satisfactorily addressed. It is also the applicant's responsibility to update any Specific Plan studies and/or provide additional studies when required by law or when determined necessary by the Planning Director.

Appendix A

A1.1: Purpose

A1. Definitions

A1.1 Purpose

This Section provides definitions of terms and phrases used in the East Gateway Specific Plan and Code that are technical or specialized, or that may not reflect common usage. If a definition in this Section conflicts with a definition in another provision of the SPMC, these definitions prevail. If a word or phrase used in the East Gateway Specific Plan and Code is not defined in this Section, or in the SPMC, the Director can determine the correct definition, giving deference to common usage.

As used in the East Gateway Specific Plan and Code, the following terms and phrases have the meaning ascribed to them, unless the context in which they are used clearly requires otherwise. All terms not defined in this Section shall be as defined in the Santa Paula Municipal Code.

A1.2 Definitions

Building: A building, in compliance with the applicable requirements of the East Gateway Code which either contains or supports the primary function/land use or the secondary/accessory function/land use on the site.

Primary Building: A building that accommodates the primary function/land use of the site.

Secondary/Accessory Building: A building that accommodates the secondary function/land use of the site.

Building Setback (Required Building Setback): The mandatory distance between a property line and a building or appurtenance. This area must be left free of structures that are higher than 3 feet, except as otherwise allowed by the zone for each block.

Build-to Line: A line appearing graphically on the development standards diagrams or stated as a required building setback, along which a building facade must be placed.

Facade: The vertical surface of a building that is set parallel to a Required Building Setback and facing a street, private street, private drive, service drive, or parking area.

Land Use(s): The individual function(s)/land use(s) on a site or within a building as allowed by Table 2 of the East Gateway Code. The following land uses are defined for the purposes of the East Gateway Specific Plan per the categories in Table 2. All other land uses are defined in the Santa Paula Municipal Code.

Agricultural Use Types

Fruit / Agriculture Stand: A freestanding structure within an allowed area of agricultural production intended for short-term use not to exceed 30 days. Examples include those temporary structures used at Farmer's Markets and fruit stands found in agricultural areas.

Automotive / Vehicle-Related Use Types

Parts, Sales without installation: The sale of automotive parts including tools and supporting fluids and cleaning products that does not include installation of any part or other product.

Parts, Sales with installation: The sale of automotive parts including tools and supporting fluids and cleaning products that includes on-site installation of any part or other product.

Repair (minor): The on-site repair of automotive vehicles that consists of repair activity that is typically concluded within a 24-hour period. Examples include 'lube and tune', window tinting, and other such activities where customers typically wait on-site for their vehicle.

Vehicle Rental: The on-site rental of automotive vehicles with most or all of the vehicle inventory for rental on-site.

Vehicle Leasing, Sales: The on-site leasing or sale of automotive vehicles with most or all of the vehicle inventory for leasing or sale on-site.

Eating / Drinking Use Types

Sidewalk Dining: Eating activity associated with a food-establishment that occurs within an adjacent sidewalk or open space such as a forecourt, dining patio or plaza.

Entertainment / Recreation Use Types

Health/Athletic Club: A fitness center, gymnasium, health and athletic club, which may include any of the following:

exercise machines, weight facilities, group exercise rooms, sauna, spa or hot tub facilities; indoor tennis, handball, racquetball, archery and shooting ranges and other indoor sports activities, indoor or outdoor pools.

Indoor Recreation Center: An establishment providing indoor amusement and entertainment services for a fee or admission charge, including:

bowling alleys, electronic game arcades (video games, pinball, etc.), ice skating and roller skating, pool and billiard rooms as primary uses.

Four or more electronic games or amusement devices (e.g., pool or billiard tables, pinball machines, etc.) in any establishment, or a premises where 50 percent or more of the floor area is occupied by electronic games or amusement devices, are considered a commercial recreation facility; three or fewer machines or devices are not considered a land use separate from the primary use of the site.

Special Event: A temporary land use such as exhibits of arts and crafts, festivals, food events, as allowed by the City through a Special Event Permit.

Medical Use Types

Medical Services, Clinic, Urgent Care. A facility other than a hospital where medical, mental health, surgical and other personal health services are provided on an outpatient basis. Examples of these uses include:

- medical offices of 5 or more licensed practitioners and/or medical specialties
- out-patient care facilities
- urgent care facilities
- other allied health services

These facilities may also include incidental medical laboratories and blood banks.

Medical Services, Doctor Office. A facility other than a hospital where medical, dental, mental health, surgical, and/or other personal health care services are provided on an outpatient basis, and that accommodates no more than four licensed primary practitioners (e.g., chiropractors, medical doctors, psychiatrists, etc., other than nursing staff) within an individual office suite. A facility with five or more licensed practitioners is instead classified under "Medical Services - Clinic, Urgent Care."

Appendix A

A1.2: Definitions

Personal Service / Financial Use Types

Establishments providing non-medical services to individuals as a primary use. Examples of these uses include:

- Child or Adult Day Care
- Clothing / Garment, Shoe, Luggage rental and repair
- Dry cleaning pick-up stores with limited equipment
- Laundromat (self-service laundries)
- Home electronics and small appliance repair
- Locksmith
- Pet grooming without boarding
- Salon, barber, beauty, and nails
- Tailor

The above uses may also include accessory retail sales of products related to the services provided.

Retail / Commercial Use Types

Retail Store: A business selling goods, wares, or merchandise directly to the ultimate consumer.

Service / Commercial Use Types

Activities and services that are provided on-site or off premises such as the following:

- Catering
- Copy center/Postal center
- Equipment rental, sales, service (rug cleaner, hand tools)
- Interior Design center
- Laboratory for medical, dental
- Photography studio, film processing
- Painting, supplies
- Publishing
- Travel / Tourism agency

Industrial Use Types

Activities that occur on the premises such as the following:

- Assembly of small scale electronics such as household appliances
- Distribution from a building that is less than 50,000 square feet in size
- Industrial and Farm vehicle equipment rental, sales and service for vehicles such as tractors, forklifts, and small construction-related tractors
- Studio where filming occurs or is supported through on-site activity or processes
- On-site office activity which is in support of any of the above activities

Other Use Types

Activities that are specialized and occur on the premises such as the following:

- Carnival
- Club, lodge, meeting hall where periodic assemblies of members are conducted

Telecommunications Facilities: Public, commercial and private electromagnetic and photoelectrical transmission, broadcast, repeater and receiving stations for radio, television, telegraph, telephone, data network, and wireless communications, including commercial earth stations for satellite-based communications. Includes antennas, commercial satellite dish antennas, and equipment buildings. Does not include telephone, telegraph and cable television transmission facilities utilizing hard-wired or direct cable connections.

Master Developer: The master developer controls or owns the site, is responsible for managing the development and disposition of the property from initiation and design of the Specific Plan that guides development for the entire site to final buildout, obtains financing and approvals, oversees site preparation and infrastructure development, controls and contracts for the phased implementation by specialized builders /developers with experience required to complete the approved plan. The Master Developer may or may not be involved in the construction of buildings, but performs design review to insure quality control of proposals by specialized builder(s)/developer(s) implementing the Specific Plan.

Mixed use: Multiple functions within the same building or the same general area through superimposition or within the same area through adjacency.

Net Floor Area: The enclosed area of a building, excluding unglazed porches, arcades and balconies.

Paseo: A pedestrian passage. Paseos are located and designed to reduce the required walking distance within a block.

Property Frontage: The interface between the facade of a building and the edges of its site with the adjacent private street, public street open space.

Property Frontage Line: The property lines of a lot fronting a street or other public way, or a park, green or paseo.

Property Frontage Type:

Shopfront and Awning: Shopfront and Awning frontages are created by inserting storefronts with substantial glazing into the ground floor facade of a building. The facade is aligned with the property line, although partially recessed storefronts, such as recessed entrances, are also common. The building entrance is at sidewalk grade and provides direct access to a non-residential ground floor use.

Gallery: Galleries are created by attaching a colonnade to a building facade that is aligned with or near the property line and typically contains ground-floor storefronts. The colonnade projects over the sidewalk and encroaches into the public right-of-way. This frontage type is ideal for retail use.

Forecourt: Forecourts are created by setting back all or a portion of a building's facade, typically the central portion. Forecourts typically provide access to a central lobby of a larger building, but may also be combined with other frontage types that provide direct access to the portions of the facade that are close to the sidewalk. Larger Forecourts may allow for vehicular access.

Residential: Premises used primarily for human habitation that contains at least 375 square feet in net area for each dwelling.

Setback: See 'Building Setback'.

Shopfront: The portion of a building at the ground floor that is made available for retail or other commercial use. Shopfronts must be directly accessible from the sidewalk, with no intervening step.

Storefront (or storefront infill assembly): The portion of a Shopfront that is composed of the display window and/or entrance and its components including windows, doors, transoms and sill pane that is inserted into the Shopfront. It does not include the wall and piers that are a part of the Shopfront Facade, in which the display window assembly is set.

Story: A habitable floor level within a building at least ten feet tall from floor to ceiling. Individual spaces, such as lobbies and foyers may exceed one story in height.

Streetscape: The combination of building facades, sidewalks, landscaping, trees, vehicular parking and travel lanes.

Land Use Element

Population

Goals	
1.3	<p>Public services, facilities, and jobs should be provided to meet the needs of population growth.</p> <p>The East Gateway Specific Plan defines flexible land use regulations to support a mix of retail, commercial and light industrial uses to provide goods, services and employment to the residents of Santa Paula and the Santa Clara River Valley.</p>
Objectives	
1(a)	<p>The extent of population growth should be based on the ability of the City to provide public services, and approved development shall include measures to mitigate the impact on these public services and facilities.</p> <p>All development within the East Gateway Specific Plan area will pay City fees and assessments to mitigate impacts on public services and facilities, as required by City ordinances and the conditions of approval. The Fiscal Impact Analysis determined that development of the East Gateway Specific Plan area with retail commercial uses would generate approximately \$1.0 million in revenue above the City's costs to provide necessary public services. This surplus will assist the City in paying costs for existing uses in the City and for new residential development resulting from population growth.</p>
1(d)	<p>The City should encourage development that will enhance the City's ability to provide the necessary public services for existing and future populations.</p> <p>The commercial and industrial development is projected to contribute more revenue to the City than will be required to provide public services related to the project. The planned uses within the East Gateway Specific Plan will provide new goods, services and jobs locally, helping to reduce sales tax leakage and to reduce the need for City and area residents to commute and shop out of the City.</p>

Public Involvement

Not Applicable	Not Applicable
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Land Use Distribution

Goals	
3.1	<p>A healthy balance of land uses and adequate land for all community needs should be provided.</p> <p>The East Gateway Specific Plan provides for the introduction of up to 360,000 square feet of retail commercial and limited industrial opportunities serving the City and surrounding area, which is currently underserved by such local uses.</p>

Appendix B: General Plan Consistency Review

Objectives	
<p>3(a) Adequate land should be provided for all needs and a healthy balance of land uses.</p>	<p>The East Gateway Specific Plan provides approximately 36.5 acres of developable land that can accommodate a range of commercial and industrial uses, including a) a regional scale retail and commercial center, b) a light industrial and research and development (R&D) employment center, c) a “mixed-use” alternative that locates commercial uses near Highway 126 and industrial uses towards the southern portion of the Plan Area.</p>
<p>3(b) The City should establish commercial and industrial jobs-to-housing that are appropriate to meet all General Plan goals.</p>	<p>The East Gateway Specific Plan provides for the introduction of up to 360,000 square feet of regional-serving retail, commercial and limited industrial opportunities aimed at providing jobs, goods and services for the local and regional population. The Market Study determined the City is not presently served by adequate regional scale commercial uses to meet the existing and projected needs of the City’s residents.</p>
<p>3(c) Compatible mixed uses should be allowed when appropriate.</p>	<p>The East Gateway Specific Plan provides for development of a range of commercial and light industrial uses that complement the existing uses in the Lemonwood Industrial Area and the planned uses in the East Area 1 Specific Plan area.</p>
Policies	
<p>3.k.k. Include areas designated for Neighborhood Commercial, General Commercial, and Commercial Office uses in the city’s land use plan. Floor-to-area ratios (FAR) set the density and intensity of these land uses. The maximum FAR for the commercial land uses shall be 0.35, except in the Downtown Design Development Report/Improvement Plan Area where an FAR of up to 1.0 is allowed and Commercial Office Uses where an FAR of 0.50 is allowed. These FARs shall also apply to mixed uses. (IM 20, 24, 29)</p>	<p>The East Gateway Specific Plan allows a maximum FAR of 0.25.</p>
<p>3m.m. Promote commercial development along arterial roads in areas that provide adequate access and parking. (IM 13, 20, 29)</p>	<p>Based on the unique location of the Plan Area – at the City’s eastern gateway – and unique characteristics – as the only large underdeveloped site in Santa Paula with direct access to SR 126 – and based on the General Plan’s designation of this area for Highway Commercial use, the Specific Plan master plans this area to support a wide range of commercial uses, with an emphasis on retail commercial uses. Parking will be located conveniently and efficiently within a pattern that emphasizes the buildings, businesses and pedestrians, encouraging – but not requiring – shoppers to patronize multiple businesses without driving and re-parking.</p>

Appendix B: General Plan Consistency Review

3.r.r.	Provide for the continuing development of the Hallock Drive area as the principal center of new car dealerships, serving local residents and the region. (IM 13, 24)	The East Gateway Specific Plan allows a wide of commercial uses, including vehicle leasing/rental, vehicle parts and sales (with and without installation), and vehicle repair.
3.s.s.	Include areas designated for Industrial Park, Light Manufacturing and Heavy Manufacturing in the City's land use plan. Floor-to-area ratios (FAR) set the density and intensity of these land uses. The FAR for the industrial land uses shall be 0.35, with industrial park designations allowing a FAR of 0.25 and light industrial designation allowing a FAR of 0.30. These FARs shall also apply to mixed uses. (IM 13, 20, 25, 29)	The East Gateway Specific Plan also allows limited industrial and manufacturing uses.

Urban Expansion

Goals		
4.1	Development and expansion should be self-supporting of the costs of its public service and infrastructure needs.	The developer of the East Gateway Specific Plan area will finance and construct all public and capital facilities improvements – including water and sewer systems, new roadways, dry utilities, and drainage – within and adjacent to the Plan Area as well as pay all required public agency fees, including fees for law enforcement, fire protection, traffic, sewer, storm drainage, community facilities, parks and schools. The Fiscal Impact Analysis determined development will generate revenues in excess of the City's cost to provide services to this area.
4.3	Development should be designed so that it can be efficiently and economically served by City services.	<p>The East Gateway Specific Plan provides detailed Infrastructure and Implementation chapters to assure the Specific Plan area supports the intensity of proposed land uses, and that on-site and off-site infrastructure systems have adequate capacity to serve the project at build out. The sewer and water master plans for the project are based on the City of Santa Paula Wastewater and Water Master Plans, which were both updated in June 2012, and are in conformance with these plans. The drainage master plan is based on the City of Santa Paula Drainage Master Plan and is in conformance with this plan.</p> <p>The development of the Santa Paula East Gateway Specific Plan will require extending infrastructure and providing services into the Specific Plan area. Basic utilities, including, water, sewer, and other utilities exist in locations adjacent to the site, and the developer of the East Gateway Specific Plan area will bear all on- and off-site costs related to those extensions of services. The Specific Plan area is located adjacent to developed areas in the City and can public services can be economically provided.</p>

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<p>4.6 Development should preserve and enhance the quality of life within the community.</p>	<p>The regional-serving development planned for the East Area Specific Plan area is projected to offer a range of economic, fiscal, and quality of life benefits to the City of Santa Paula. The Plan not only provides for local access to a range of shopping and commercial uses for which residents currently must travel to other cities, it is also projected to generate significant revenue for the City of Santa Paula. Shoppers are anticipated to include local residents who will be able to shop locally, but residents from outside the City who will choose to shop here.</p>
<p>4.7 Development should sustain and enhance the economic health of the community.</p>	<p>The commercial and industrial development within the East gateway Specific Plan is projected to reduce sales tax leakage from the City of Santa Paula, attract sales tax revenue from out of town shoppers, and provide a range of new local jobs.</p>
<p>4.9 Development should be compatible with and have minimal adverse impacts upon the environment, agriculture and natural resources and should not be wasteful of scarce land.</p>	<p>In order to take advantage of the regional commercial development potential of this unique site - the only large potential commercial site in Santa Paula with direct access to Highway 126 - the East Gateway Specific Plan area includes land already within the City as well as land to be annexed. The land to be annexed expands the site to a size that is capable of accommodating a regional commercial center program to accomplish the objective of increasing the range of locally available goods and services. The design and policies of the Plan also support the conservation of natural resources through the appropriate design of the permanent urban edges abutting the Santa Paula/Fillmore Greenbelt and Santa Clara River.</p>
<p>Policies</p>	
<p>4.b.b. Emphasize infill prior to annexations. (IM 31, 38, 44, 45)</p>	<p>The limited annexation that would be associated with the adoption of the Specific Plan - and the commercial development which it would permit - is necessary to consolidate a site large enough to accommodate the program for such a regional-serving commercial center.</p>
<p>4.d.d. Annex and develop the contiguous lands first. (IM 32, 33, 34, 35, 36)</p>	<p>The Specific Plan site is immediately contiguous to land already within the City limits, and is in fact the balance of a single parcel much of which already lies within the City limits.</p>
<p>4.g.g. Minimize public expenditures for services and infrastructure needed by new land development projects through the use of owners associations, private facilities, and project designs that minimize costs. (IM 44, 45, 46)</p>	<p>The developer of the East Gateway Specific Plan will finance and construct all public and capital facilities improvements – including water and sewer systems, new roadways, dry utilities, and drainage – within and adjacent to the Plan Area as well as pay all required public agency fees, including fees for law enforcement, fire protection, traffic, sewer, storm drainage, community facilities, parks, and schools.</p>
<p>4.r.r. Encourage a mix of service and retail commercial</p>	<p>The East Gateway Specific Plan provides approximately 300,000</p>

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<p>uses in the East 2 and West 2 planning and expansion areas to provide services for employees in the area. (IM 41, 43)</p>	<p>to 360,000 square feet of commercial and limited light industrial space within East Area 2 in a location that is easily accessible from the entire City and surrounding region. The Specific Plan allows a full range of service and retail commercial uses.</p>
<p>Implementation Measures</p>	
<p>41. The following Development Standards for the East Area 2 planning area may, as determined by the Planning Director, be implemented through a Specific Plan or Master Plan and subsequent development approvals:</p> <ul style="list-style-type: none"> • <i>The car sales lots should not dominate the scene. Create the “virtual” car sales area.</i> • <i>Establish a design theme for the area. This theme should consider Santa Paula’s existing outstanding architecture and capitalize on an agricultural theme and small town character.</i> • <i>Reduce large parking lots. Hide parking between and behind buildings. Use smaller, scattered parking lots.</i> • <i>Maintain non-urban space and proportions.</i> • <i>Provide setbacks with tree-lined streets.</i> • <i>Development shall incorporate architectural details on buildings fronting SR 126.</i> • <i>Provide landscape screening for buildings and parking lots.</i> • <i>Development shall be pedestrian friendly. A mix of uses should be located within walking distance of each other.</i> • <i>Development shall be designed to fit in the context of what defines Santa Paula: small scale, historical detailing.</i> • <i>Require new lighting that is part of any proposed development to be oriented away from sensitive uses, and shielded to the extent possible to minimize glare and spill over.</i> 	<p>The East Area Gateway Specific Plan implements these Development Standards by introducing buildings that reflect Santa Paula’s small-town scale and face streets with welcoming frontages and entries scaled to the pedestrian. Buildings along SR 126 are smaller than buildings farther from the highway, freestanding with four-sided architectural character, and with natural and agriculturally-oriented landscape interspersed to accent the buildings.</p> <p>Streets are balanced and scaled to their context and role for use by motorists while creating a low-speed environment suitable for pedestrians and bicycles.</p> <p>The East Gateway Specific Plan provides for the introduction of up to 360,000 square feet of commercial, retail and limited industrial opportunities aimed at serving both the local and regional population.</p> <p>Parking is located conveniently and efficiently within a pattern that emphasizes the buildings, businesses and pedestrians, encouraging - but not requiring - shoppers to patronize multiple businesses without driving and re-parking. Views of parking areas are buffered with landscaped edges that include trees, shrubs and low walls or fences.</p> <p>Landscaping emphasizes the private street and drive frontages with tree rows, with randomly located larger trees distributed within many parking areas - rather than the more conventional pattern of many smaller trees - reflect a more rural landscape character.</p> <p>Site, building, and signage lighting prevents offsite glare by shielding lamps with permanent attachments to light fixtures and positioning lamps within 20 feet of the ground.</p>
<p>42. Development shall emphasize the small village concept with small buildings.</p>	<p>Buildings along the 126 highway frontage - a primary gateway to the City of Santa Paula - are required to be designed as town-scale buildings with a unique small-town design character.</p>

Urban Form and Design

<p>Goals</p>		
<p>5.1 The small town atmosphere of Santa Paula and the</p>	<p>The East Gateway Specific Plan promotes the design of buildings</p>	

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<p>quality of life in the community should be preserved and enhanced.</p>	<p>that reflect Santa Paula's town-scale, agricultural roots and physical character through appropriate building scale, massing, height, form and character. Such buildings are required along the primary highway/gateway frontage of Highway 126. For larger buildings in the southerly blocks, such scale and character is provided primarily with facade articulation facing toward the highway.</p>
<p>Objectives</p>	
<p>5(b) Neighborhood commercial businesses (markets, dry cleaners, etc.) should be encouraged in all new residential and industrial development of substantial size.</p>	<p>The East Gateway Specific Plan accommodates a variety of neighborhoods-serving businesses, including cafes, delicatessens, restaurants, and bakeries; grocery stores; pharmacies; banks, savings and loans, and credit unions; barber, beauty, and nail salons; dry cleaners and laundromats; hardware; clothing; pet supplies and grooming; leather, luggage, shoe, electronics, and small appliance repair shops. These uses are intended to serve the new residential development planned for the adjacent East Area 1 and East Area 2, as well as the City as a whole and the surrounding area.</p>
<p>5(f) Sufficient land should be provided for all uses, including parks, low-density residential, industrial and neighborhood commercial, to accommodate projected population growth to the year 2020.</p>	<p>The East Gateway Specific Plan provides land to accommodate a wide range of commercial and light industrial uses.</p>
<p>5(h) The City should develop signs, monuments or other physical features that announce the entrance to the City and/or the downtown.</p>	<p>The East Gateway Specific Plan establishes a strong eastern gateway for Santa Paula by establishing a "gateway commercial" character along Highway 126 as it passes through the Plan Area. This is accomplished through a combination of landmark elements and buildings, and bold but appropriate commercial and civic signage and graphics that relate to Santa Paula's agricultural roots and physical pattern, scale and character. In addition, the gateway triangle block north of Highway 126 is defined by a drainage swale landscaped with native plants, river rock and a low open fence to resemble a dry creek bed, with a unique sign at the point of the site welcoming visitors to Santa Paula.</p>
<p>5(m) The City should protect and expand the urban forest.</p>	<p>The East Gateway Specific Plan provides a landscape regulating plan that emphasizes large trees characteristic of the Santa Clara River valley. The highway frontage as well as the private street and drive frontages are defined with strong tree rows, and picturesquely located larger trees are distributed within parking areas – rather than the more conventional pattern of smaller trees – to reflect a rural landscape character for this edge of town gateway site.</p>

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Policies	
<p>5.c.c. Provide for distinctive and compatible residential neighborhoods and commercial and industrial districts. (IM 48, 49, 50, 51, 52, 53, 54, 55, 56)</p>	<p>The East Gateway Specific Plan is a commercial/light industrial district that is flexible in use and reflective of Santa Paula's town-scale, agricultural roots and physical character through appropriate building scale, height, form and character.</p>
<p>5.f.f. Require that techniques be used to avoid "box-like" commercial structures, including: differentiation of facades and elevations, articulation of building details (roof, columns, beams, balconies, arcades, trellises, recessed windows, etc.). (IM 54, 56)</p>	<p>Buildings and frontages along Highway 126 are smaller than buildings farther from the highway, freestanding with four-sided architectural character, and with natural and agriculturally-oriented landscape interspersed to accent the buildings. Larger buildings within the southerly blocks of the site will be provided with articulated facades and rooflines.</p>
<p>5.g.g. Require that street-facing building facades have usable occupiable space and entries. (IM 54, 56)</p>	<p>The East Gateway Specific Plan requires that buildings must incorporate active, occupied frontage types, including shopfronts, galleries, and forecourts, into any facade that faces SR 126, a private interior street or, a parking area.</p>
<p>5.h.h. Require that non-residential development be designed to orient outward to pedestrian sidewalks, parking lots and public streets. (IM 54, 56)</p>	<p>Buildings - whether retail, service commercial or research and development in program - are required to face adjacent streets, drives and parking areas with windows and entries accessible from adjacent sidewalks.</p>
<p>5.i.i. Require that non-residential development be designed to a "human scale" at the street/sidewalk/parking elevations. (IM 54, 56)</p>	<p>The commercial and/or light industrial buildings of the East Gateway Specific Plan area are required to face streets and parking areas with human scale facades that are articulated with windows, entries and specific frontage types to welcome the pedestrian.</p>
<p>5.j.j. Require that non-residential development outside of the Downtown Design Development Report/Improvement Plan Area convey a rural "village-like" environment. (IM 54, 56)</p>	<p>The building standards and guidelines of the East Gateway Specific Plan are crafted to require exactly such a rural, small-town design character through their massing, articulation, and material palette.</p>
Implementation Measures	
<p>54. Establish design standards for various areas (downtown, railroad corridor, city entrances/gateways, Harvard Blvd.) and provide for design review and approval for new development and remodeling of existing buildings in these areas in accordance with the Special Study Areas implementation measures included below.</p>	<p>The East Area Specific Plan provides design standards for all development within the Plan Area.</p>

Land Use Compatibility

Goals	

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<p>6.3 Mixed land uses that are compatible should be allowed.</p>	<p>The East Gateway Specific Plan provides land use and urban design standards to allow a range of regional and local-serving retail and commercial businesses to be operated within close proximity to and within walking distance of other office, light manufacturing and employment uses.</p>
<p>6.5 Development should mitigate undue generation of noise and light.</p>	<p>The limited industrial uses allowed within the East Gateway Specific Plan do not include indoor or outdoor manufacturing uses or operations that would have the potential to generate undue noise and light exposures. Site lighting – including illumination of parking areas, buildings, pedestrian routes, dining areas, design features and public sidewalks or open spaces – to prevent offsite glare by shielding lamps with permanent attachments to light fixtures and to position lamps within 20 feet of the ground. In addition, the East Gateway Specific Plan requires that sign lighting be designed to minimize light and glare on surrounding rights-of-way and properties.</p>
<p>6.6 Development should mitigate undue exposure of citizens to existing noise and light sources.</p>	<p>The limited industrial uses allowed within the East Gateway Specific Plan do not include indoor or outdoor manufacturing uses or operations that would have the potential to generate undue noise and light exposures. Site lighting – including illumination of parking areas, buildings, pedestrian routes, dining areas, design features and public sidewalks or open spaces – to prevent offsite glare by shielding lamps with permanent attachments to light fixtures and to position lamps within 20 feet of the ground. In addition, the East Gateway Specific Plan requires that sign lighting be designed to minimize light and glare on surrounding rights-of-way and properties.</p>
<p>Objectives</p>	
<p>6(a) The City should encourage low intensity land uses and/or barriers near agricultural lands.</p>	<p>The East Gateway Specific Plan locates parking areas and one-story buildings along the edge of the site abutting agricultural land, and provides landscape buffers of non-invasive and native species to define a stable, permanent urban edges abutting the Santa Paula/Fillmore Greenbelt and Santa Clara River.</p>
<p>6(b) Industrial areas should be buffered from surrounding land uses.</p>	<p>The industrial uses allowed by the East Gateway Specific Plan are limited to very light uses that are compatible with adjacent retail and office commercial uses, requiring no buffering other than that provided by appropriate site and building design.</p>

Economic Development

<p>Goals</p>	
<p>7.1 The creation of jobs should be promoted within the community.</p>	<p>The commercial goods and services and light industrial uses within the East Gateway Specific Plan area will provide new local jobs within Santa Paula, helping to improve the City's jobs/housing balance and expand the local economy.</p>
<p>7.3 Diversity and self-sufficiency should be promoted in the local economy.</p>	<p>The commercial goods and services and light industrial uses within the East Gateway Specific Plan area provide new local jobs and sales tax generation to improve the City's fiscal sustainability, jobs/housing balance, and local economy.</p>
<p>Objectives</p>	

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7(h)	The attraction and retention of commercial and industrial businesses should be aggressively pursued	The East Gateway Specific Plan represents a unique opportunity to provide facilities for a segment of commercial businesses not currently available within the City, as well as other light industrial and office based businesses seeking a prominent regional address with nearby commercial services and amenities.
7(k)	Provide new attractions and commercial draws for tourists and residents.	The East Gateway area is planned to provide a range of shopping and dining opportunities that are equally visible, accessible and attractive to local residents and passing tourist. As the only such property in Santa Paula directly facing toward and accessible from Highway 126 the East Gateway is uniquely positioned to help Santa Paula achieve this objective.

Infrastructure

Goals		
8.2	The orderly continuation, upgrading and expansion of utility services should meet community needs and accommodate new technologies. (Utilities: electricity, telephone, cable TV, gas, water, sewer, petroleum, solid waste, and street lights.)	The East Gateway Specific Plan assures the Plan Area is developed in a way that supports the intensity of proposed land uses with appropriate on-site and off-site infrastructure systems with adequate capacity to serve the permitted intensity of use.
Objectives		
8(a)	A system of impact fees and/or development agreements should be adopted to assess land development projects for the costs of public facilities, utilities, and infrastructure needed to serve such projects, including but not limited to the following: fire, police, roads, sewers, flood control, recreation, and water.	The developer of the East Gateway Specific Plan area will finance and construct all public and capital facilities improvements – including water and sewer systems, new roadways, dry utilities, and drainage – within and adjacent to the Plan Area as well as pay all required public agency fees, including fees for law enforcement, fire protection, traffic, sewer, storm drainage, community facilities, parks and schools. A development agreement is also being approved with the property owners.
8(h)	Where feasible, new and existing utility systems should be undergrounded.	All new local serving electrical lines will be placed underground, with conduits in full encasement.
8(i)	The City should require installation of all utility systems as part of land development projects.	The developer of the East Gateway Specific Plan area will finance and construct all public and capital facilities improvements – including water and sewer systems, new roadways, dry utilities, and drainage – within and adjacent to the Plan Area as well as pay all required public agency fees, including fees for law enforcement, fire protection, traffic, sewer, storm drainage, community facilities, parks and schools.
Policies		

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<p>8.b.b. Have development pay the costs of needed utility services. (IM 107, 108, 109, 110)</p>	<p>The developer of the East Gateway Specific Plan will finance and construct all public and capital facilities improvements – including water and sewer systems, new roadways, dry utilities, and drainage – within and adjacent to the Plan Area as well as pay all required public agency fees, including fees for law enforcement, fire protection, traffic, sewer, storm drainage, community facilities, parks and schools.</p>
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Special Study Areas

Policies	
<p>9.r.r. Promote aesthetics and visibility for the City's freeway and arterial gateways. (IM 122, 123, 124)</p>	<p>The East Gateway Specific Plan establishes a “highway gateway” character along Highway 126 as it passes through the Plan Area through the use of landmark elements and buildings, and along with bold but appropriate commercial and civic signage and graphics that relate to Santa Paula's agricultural roots and physical pattern, scale, and character.</p>
Implementation Measures	
<p>122. Design standards shall be developed for new development, signage and public improvements to be located at the entrances to the City. At a minimum, the standards shall apply to the following locations: the Santa Paula Freeway ramps, Main Street, Telegraph Road, Harvard Boulevard, and Ojai Road)</p>	<p>The East Area Specific Plan provides design standards for new development – including buildings, signage, landscape, and street design – that along these uniquely visible Highway 126 frontages, and at its intersection with Hallock Drive, the one such intersection in the City of Santa Paula.</p>
<p>123. The following design principles shall be used for the City gateway areas:</p> <ul style="list-style-type: none"> • The east and west gateways shall acknowledge and provide a differentiation between the rural and urban environment. • The freeway gateways shall have a unifying visual identity program that acknowledges arrival and directs visitors. Features of natural stone, wood, and brick are appropriate for this civic sign/identity programs. 	<p>The East Gateway Specific Plan establishes the “highway gateway” character along Highway 126 as it passes through the Plan Area through the use of landmark elements and buildings, and along with commercial and civic signage and graphics that relate to Santa Paula's agricultural roots and physical pattern, scale and character. In addition, the gateway triangle block north of Highway 126 is defined by a drainage swale that will be landscaped with native plants, river rock and a low open fence to resemble a dry creekbed, with a unique sign at the point of the site welcoming visitors to Santa Paula.</p>

Circulation Element

Streets and Highways

Goals	
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1.1	Provide for the long-range development of Santa Paula's roadway system consistent with adopted land use patterns.	The private street and block network of the East Gateway Specific Plan extends the simple grid structure and small-town character of Santa Paula.
1.2	Ensure the safe and efficient movement of people and goods.	The public and private streets within the Plan area accommodate various transportation modes, including motor vehicles, bicycles, and pedestrians.
1.4	Implement funding strategies for construction, improvement, and maintenance of existing and new roadways.	The developer of the East Gateway Specific Plan area will finance and construct all new roadways within the Plan Area as well as pay all required public agency fees, including fees for law enforcement, fire protection, traffic, sewer, storm drainage, community facilities, parks and schools.
Policies		
1.b.b	Sufficient funding should be obtained to properly maintain, operate, and improve the public street system. (IM 5, 6, 7)	The developer of the East Gateway Specific Plan will finance and construct all new roadways within the Plan Area as well as pay all required public agency fees, including fees for law enforcement, fire protection, traffic, sewer, storm drainage, community facilities, parks and schools.
1.d.d	The City should place a high priority on safety and the identification and elimination of high accident locations. (IM 4)	The intersection of Hallock Drive and SR 126 will be improved to accommodate anticipated traffic volumes, while the main entry point to the Plan Area will be coordinated with Caltrans to ensure that it meets Caltrans' design standards, including the distance from Hallock Drive. In addition a sidewalk and bikeway separated by a parkway planted with street trees and native California plantings along SR 126 improves pedestrian and bicycle safety by separating pedestrians and bicycles from SR 126. Finally, the Plan Area's internal streets provide sidewalks separated from the street by parkways planted with street trees.

Public Transportation

2.1	Safe, convenient, efficient, and accessible transportation should be available to everyone.	The East Gateway Specific Plan provides a pedestrian circulation network throughout the Plan area, and several locations that would be suitable for the insertion of bus stops - primarily on Street B in the northerly half of the Plan area – if bus service to the site is provided.
2.2	Reliance on single-occupancy motor vehicles should be reduced and utilization of public transit and alternative transportation modes should be increased.	In addition to the pedestrian network to support future transit service, the Plan is connected to the local and regional bike network with a trail along the north edge of the site, and connecting to the internal circulation system of the East Gateway.

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Aviation

<p>3.4 Development should be compatible with existing risks from aviation.</p>	<p>The East Gateway Specific Plan area is not located within identified areas of risk related to the Santa Paula Airport.</p>
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Transportation Management Control

Goals	
<p>4.1 Traffic congestion and air pollution should be reduced by decreasing the number and length of motor vehicle trips.</p>	<p>The East Gateway Specific Plan provides convenient commercial goods and services within Santa Paula, so that residents will need to make fewer trips to other cities to shop or work, thereby reducing the length of many motor vehicle trips. In addition, a shared parking approach, as described below, encourages patrons to walk between businesses and destinations, further reducing the number of motor vehicle trips.</p>
<p>4.3 Development should mitigate undue pollution and/or congestion.</p>	<p>Shorter motor vehicle trips (as described above) and a park-once strategy (as described below) can lower vehicle emissions, while helping reduce congestion.</p>
<p>4.4 Adequate parking and access should be provided in commercial areas.</p>	<p>Parking requirements necessary to support the intended program within the East Gateway Specific Plan area conform to City parking ratios. In addition, the East Gateway Specific Plan utilizes a shared parking approach whereby each property is allowed to group parking facilities throughout the plan area in order to make efficient use of parking facilities and to encourage walking between businesses and destinations by relieving individual properties of providing potentially duplicative parking throughout the plan area.</p> <p>Individual businesses may choose to either provide parking on their individual building site or to share the available parking spaces within the code boundaries. Compliance with the aggregate amount of required parking by all of the individual businesses in the plan area must be monitored by the City for land use compatibility and the City must determine the method(s) and level of information necessary to monitor parking.</p>
Objectives	
<p>4(a) The growth of the number and length of motor vehicle trips should be reduced.</p>	<p>The East Gateway Specific Plan provides convenient commercial goods and services within Santa Paula, reducing the need for many out-of-town trips. Further, the East Gateway is located immediately adjacent of East Area 1 - the location of much of the intended growth of Santa Paula over the next 10 to 20 years – providing goods and services to those residents with vehicle trips of less than a mile, which will facilitate bicycle or pedestrian trips.</p>

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Policies		
4.d.d	<p>New developments should provide parking according to code requirements. (IM 20)</p>	<p>Parking requirements necessary to support the intended program within the East Gateway Specific Plan area conform to City parking ratios.</p>

Non-Motorized Transportation

Goals		
5.1	<p>City streets should be designed for safe bicycle, pedestrian, and equestrian use.</p>	<p>A sidewalk and bikeway separated by a parkway planted with street trees and native California plantings along SR 126 improves pedestrian and bicycle safety by separating pedestrians and bicycles from SR 126. In addition, the Plan Area's internal streets provide sidewalks separated from the street by parkways planted with street trees.</p>
5.2	<p>An off-street bicycle and pedestrian corridor should be provided to serve major points of interest in town and beyond.</p>	
Objectives		
5(a)	<p>The City should de-emphasize street bike lanes in favor of a policy that encourages street design for all methods of travel (car, bikes, pedestrians, and equestrian).</p>	<p>The internal private streets within the East Gateway Specific Plan area are designed for low speed vehicular traffic that is compatible with bicycle traffic and a safe and comfortable pedestrian environment.</p>
Policies		
5.e.e	<p>Sidewalks and pedestrian ways in new development should be carefully designed to remain clear of obstructions, have appropriate grades, and be accessible in order to encourage pedestrian use. (IM 25, 26)</p>	<p>Sidewalks are provided along SR 126 and along all internal streets and will be free of pedestrian obstructions. Utility equipment will not be located in sidewalks.</p>
5.g.g	<p>New commercial and industrial developments should provide well-designed, convenient pedestrian and bicycle parking facilities. (IM 26)</p>	<p>The East Gateway Specific Plan requires one bicycle stall be provided for every 50 automobile parking stalls; that bicycle parking must be distributed to serve all buildings; and that bicycle parking must be within 50 feet of a building entrance, be adequately lighted, and provide adequate clearance around the facility to give users room to maneuver.</p>

Conservation and Open Space Element

General

Goals	
<p>1.2 Hazards to natural resources should be controlled or eliminated, including but not limited to:</p> <ul style="list-style-type: none"> invasive non-native plants and animals pollution incompatible activities or land uses 	<p>The East Gateway Specific Plan includes landscape standards for native and/or non-invasive plant species in general, and particularly along the east and south edges of the site abutting natural and agricultural open spaces. These edges are also required to provide bio-filtration of stormwater to avoid pollution of those open spaces. The land use and design standards ensure compatibility of adjacent land uses, existing and future.</p>

Open Space

Goals	
<p>2.1 Preserve open spaces for future generations.</p>	<p>The East Gateway Specific Plan defines a stable, permanent southeast corner for the City of Santa Paula, thus defining and preserving the City boundary with the Santa Clara River to the south and the Santa Paula/Fillmore Greenbelt to the east.</p>
<p>2.4 The Santa Clara River and Santa Paula Creek should be treated as important assets to be conserved and more public open space opportunities should be provided.</p>	<p>The East Gateway Specific Plan provides an urban edge shopping and working environment with views of the Santa Clara River open space and the hills beyond, while protecting those resources with appropriate edge designs.</p>
Objectives	
<p>2(c) Encourage that development which is designed in a manner sensitive to the natural features of the site and to the surrounding character</p> <p>2(e) Heighten the sense of nature in the City.</p>	<p>The East Gateway Specific Plan introduces a number of landscape features that introduce nature into the City:</p> <ul style="list-style-type: none"> The Plan is organized to provide views of the adjoining natural and agricultural open spaces. A landscaped highway edge setback characterized by native California plantings in an informal, rural composition of drought tolerant ground covers, perennials is introduced along SR 126. Large trees vertically define the highway edge while providing clear views of buildings below their canopies. The triangular parcel fronting SR 126 as the visitor enters Santa Paula is defined by a drainage swale that is landscaped with native plants, river rock and a low open fence to resemble a dry creekbed, with a unique sign at the point of the site welcoming visitors to Santa Paula.

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Policies	
2.c.c. Provide for open space amenities in all new residential, commercial, and industrial development. (IM 1,2,5,6,7,9,11)	Outdoor dining areas or other usable courts or gardens are sited between or adjacent to the buildings along SR 126, designed with shade trees, fruit trees, and seating areas for outdoor dining in a comfortable setting similar to so many roadside stops that were characteristic of the region.

Agriculture and Soils

Goals	
3.2 Development should be compatible with and have minimal adverse impacts upon agriculture and natural resources and should not be wasteful of scarce land.	The East Gateway Specific Plan guides the Design buildings that reflect Santa Paula's town-scale, agricultural roots and physical character and the landscape along the Specific Plan area's edges is rural and natural in character, emphasizing native and non-invasive plant materials that create a seamless transition to the adjoining natural and agricultural environments.
3.3 Urban expansion should be directed away from the most productive agricultural areas.	The City has established large agricultural greenbelts to the west and east of the City to protect the most productive agricultural land in the City's Planning Area. The East Gateway Specific Plan area is located within the East Area 2 Planning area, which is identified for urban development in the General Plan.
Objectives	
3(a) Encourage low intensity land uses and/or barriers near agricultural lands.	The East Gateway Specific Plan limits development to an FAR of 0.25 and building eave heights of between 25 and 40 feet, with well-designed buffers adjacent to the agricultural east edge of the Plan.

Air Quality

Goals	
4.1 The improvement and protection of air quality should be encouraged and supported.	By providing local shopping and employment opportunities not previously available in Santa Paula, the East Gateway Specific Plan is projected to significantly reduce the length of many vehicle trips, thereby improving and protecting air quality. By providing a robust pedestrian circulation system and shared parking facilities, the Plan is anticipated to further reduce the number of vehicle starts and short vehicular trips that often occur within commercial centers that lack such amenities.

Biological Resources

Goals	
5.2	<p>Development should be compatible with and have minimal adverse impacts upon the environment and natural resources and should not be wasteful of scarce land.</p>
	<p>Buildings and streets near the plan area's edges utilize special details for connecting the development sensitively with the adjacent areas to be left in a natural state. Landscape along these edges is rural and natural in character, emphasizing native and non-invasive plant materials that create a seamless transition to the adjoining natural and agricultural environments. The landscape palette along Highway 126 also emphasizes native plants to help integrate the new "edge of town" development into the adjoining rural landscapes.</p>
5.3	<p>Hazards to natural resources should be controlled or eliminated, including but not limited to:</p> <ul style="list-style-type: none"> • invasive non-native plants and animals • pollution • incompatible activities or land uses
	<p>The East Gateway Specific Plan requires landscape to contribute to mitigating environmental degradation resulting from storm water runoff by implementing landscape design methods that reduce runoff and improve water quality by cleansing and detaining storm water on site by utilizing a combination of biofiltration, permeable paving and surface detention methods.</p>
5.7	<p>The urban forest should be protected and enhanced</p>
	<p>The East Gateway Specific Plan provides landscape standards that emphasize abundant trees throughout.</p>
Objectives	
5(c)	<p>Site and develop land uses to minimize the impacts on sensitive biological resources.</p>
	<p>Buildings and streets near the plan area's edges utilize special details for connecting the development sensitively with the adjacent areas to be left in a natural state. Landscape along these edges is rural and natural in character, emphasizing native and non-invasive plant materials that create a seamless transition to the adjoining natural and agricultural environments.</p>
Policies	

Cultural/Historic Resources

Goals	
6.2	<p>The historic, cultural and archaeological resources of the community should be preserved for future generations.</p>
	<p>No identified cultural or historic resources are present on the East Gateway Specific Plan area.</p>

Energy Conservation

Goals	
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Appendix B: General Plan Consistency Review

<p>7.1 The orderly continuation, upgrading and expansion of utility services should meet community needs and accommodate new technologies.</p>	<p>The East Gateway Specific Plan ensures that the site is developed in such a way as to support the intensity of permitted land uses, and that on-site and off-site infrastructure systems have adequate capacity to serve the project at build out. The development of the Santa Paula East Gateway Specific Plan requires extending infrastructure and basic utilities – including water, sewer, electricity, communication and gas – into the Specific Plan area. New local serving electrical and communication lines will be placed underground, with conduits in full encasement. Gas service connections will be provided and maintained throughout the Specific Plan area as needed.</p>
<p>Objectives</p>	
<p>7(b) Energy and water conservation should be promoted as part of land development projects.</p>	<p>The Santa Paula East Gateway Specific Plan includes energy conservation related design standards to reduce electric energy consumption. In addition, to minimize exterior water use, the East Gateway Specific Plan requires the use of drought resistant species and water conserving native plants; low precipitation rate irrigation; and the use of automatically controlled irrigation systems regulated to the actual evapotranspiration rate of the soil.</p>
<p>Policies</p>	
<p>7c.c. Energy and water conservation should be promoted as part of land development projects. (IM 35)</p>	<p>The Santa Paula East Gateway Specific Plan includes energy conservation related design standards to reduce electric energy consumption. In addition, to minimize exterior water use, the East Gateway Specific Plan requires the use of drought resistant species and water conserving native plants; low precipitation rate irrigation; and the use of automatically controlled irrigation systems regulated to the actual evapotranspiration rate of the soil.</p>

Mineral Resources

<p>Goals</p>	
<p>8.2 Reduce incompatibilities between mineral extraction areas and other land uses.</p>	<p>No mineral extraction operations or facilities are present on within or adjacent to the East Gateway Specific Plan area.</p>

Water Resources

<p>Goals</p>	
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Appendix B: General Plan Consistency Review

9.2	Existing risks from floods should be reduced.	The existing site is approximately four feet lower than Highway 126, slopes southerly towards Santa Clara River, and is currently within the Base Flood Elevation of Orcutt Creek. The master drainage plan in the Specific Plan identifies improvements that will mitigate the existing flooding risk.
9.4	Waterways should be protected from pollutants, soil erosion and other environmental hazards.	The East Gateway Specific Plan requires landscape to contribute to mitigating environmental degradation resulting from storm water runoff by implementing landscape design methods that reduce runoff and improve water quality. Potential construction impacts will be mitigated through compliance with applicable City and County ordinances and City conditions of approval.
Objectives		
9(b)	New construction and substantial improvements to existing construction should comply with the City's floodplain management ordinance.	New development within the East Gateway Specific Plan area will comply with the City's floodplain management ordinance.
Policies		

A.3.10 Scenic Resources

Goals		
10.1	Scenic views and vistas, tree-lined streets, open spaces, natural areas, ridgelines and land forms should be preserved.	The development and design standards of the East Gateway Specific Plan are specifically intended to ensure that this important edge of town gateway area – which will permanently define the eastern entry to the City of Santa Paula – presents a unique image and vistas that define the transition from the small town environment of Santa Paul to the rural environment of the Santa Paula/Fillmore Greenbelt.

A.3.11 Parks and Recreational Resources

Goals		
11.1	Park and recreation areas and programs, open spaces, natural areas, civic and cultural resources should be provided to meet the needs of all age groups and members of the public.	As a commercial/industrial district, the East Gateway Specific Plan area is not required to provide on-site park space. However the Plan does provide standards for publicly accessible open spaces associated with restaurant uses in the northerly portion of the Plan area.

A.4 Noise Element

Appendix B: General Plan Consistency Review

There are no residential uses or other sensitive noise receptors within or near the East Gateway Specific Plan area. Development of commercial and light industrial uses will be consistent with the policies in the Noise Element.

A.5 Safety Element

No significant safety issues or impacts were identified in the EIR prepared for the East Gateway Project that will not be mitigated by developing in accordance with existing development standards and regulations. For this reason, the East Gateway Specific Plan is consistent with the Safety Element.